



Development Report

# Thrive - Smithville West, Town of West Lincoln

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Application for Zoning By-law Amendment, Draft Plan of Subdivision & Future Draft Plan of Common Element Condominium



Prepared for Marz Homes (Smithville West Inc.)  
by IBI Group  
July 16, 2020

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# 1 Introduction

Marz Homes (Smithville West) Inc. (“the Owner”) is the owner of lands legally referred to as Lot 9 and Plan M98, as confirmed by Plan 30BA1695 in the Township of West Lincoln, in the Regional Municipality of Niagara (the subject lands”). The subject lands are approximately 10.3 hectares in size and are currently vacant. It is the owner’s intent to develop the subject lands into ‘THRIVE’, a proposal of approximately 224 residential units. Further broken down as follows; 45 single detached units, 23 bungalow townhouse units, 28 back-to-back townhouses, 21 three-storey townhouse units, 75 two-storey townhouses and a maximum of 32 mixed use residential units. Ground floor commercial is proposed within the three-storey mixed use building. The proposed development will also include a storm-water management pond, open space and trails.

In 2015, the subject lands were brought into the Urban Boundary of Smithville, through the Town of West Lincoln’s Official Plan Amendment 37. In 2017, the North-west Quadrant Secondary Plan was approved, which identified areas for low, medium and high density development as well as local commercial development. An Official Plan Amendment (“OPA”) is not required, as the Secondary Plan allows for flexibility as long as the intent of the plan is maintained. The proposed development simply changes the placement of the land use designations identified, to achieve a higher sense of compatibility, thereby maintaining the intent of the Secondary Plan.

The proposed development requires a Zoning By-law Amendment (“ZBLA”), Draft Plan of Subdivision and Common Element Condominium approval to permit the proposed uses on the subject lands, as well to implement appropriate provisions to regulate development. To facilitate the proposed residential development, a ZBLA is required to rezone a portion of the subject lands from the Development ‘D’ Zone to modified Residential Low Density ‘R3’, Residential Medium Density – Type 3 ‘RM3’ and Service Commercial ‘C3’ Zones. It is also required to rezone the open space uses from Development “D” Zone to Open Space “OS” Zone.

IBI Group has been retained by the Owner to provide an independent professional planning opinion, in addition to the coordination and submission of the complete planning applications. IBI Group supports the proposed applications, as they are reflective of good planning. This report provides a thorough analysis of current and applicable Provincial legislation and plans, local planning documents, supporting studies, technical works, and other relevant matters in context of the proposed development.

## 2 Site Description and Surrounding Uses

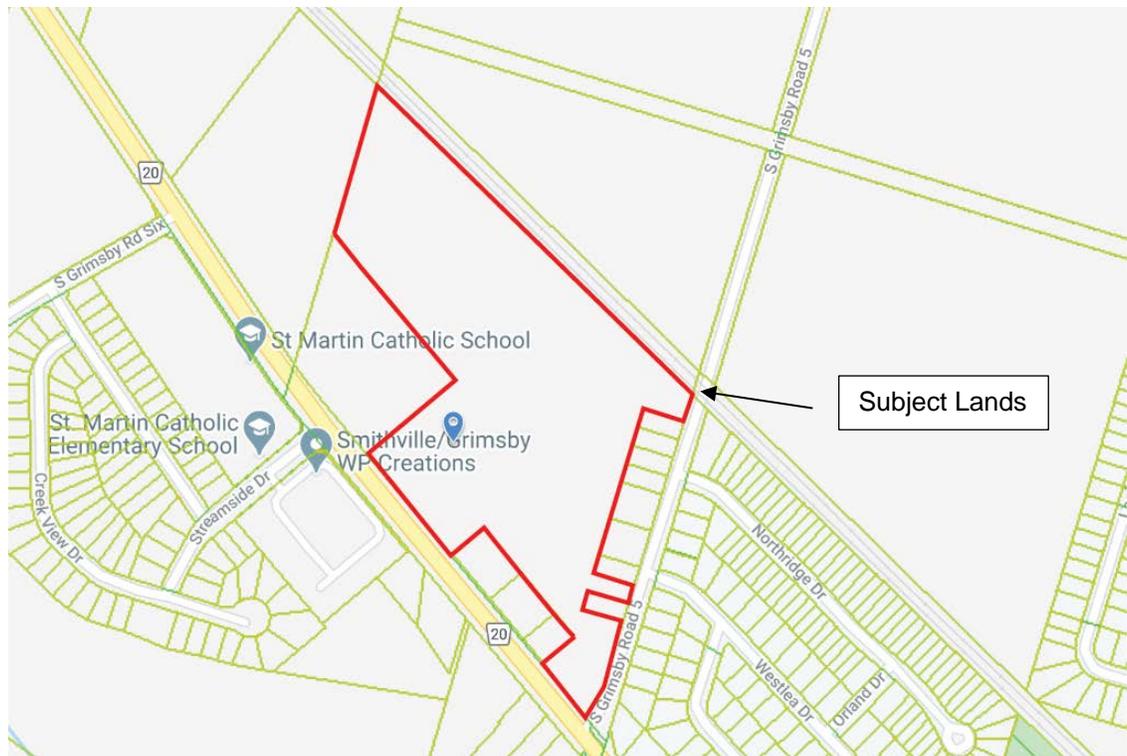
The following subsections provide a review of the context, existing uses and conditions of the subject lands and surrounding area. These elements frame the discussion on the proposed planning applications, as well as the planning comments and justification.

### 2.1 Site Location and Description

The subject lands are legally described as Lot 9, and Plan M98 as confirmed by Plan 30BA1695 (Geographic Township of West Lincoln). The lands are bound by West Street (Regional Road 20) to the south, South Grimsby Road 5 to the east, the CP Rail Line to the north and agricultural and institutional uses to the west. The subject lands have a lot frontage of approximately 159 metres along Grimsby Road 5 and 211 metres along West Street, with an area of 103, 517 square metres (10.3 hectares).

As previously mentioned, the subject lands are currently vacant and can be characterized as open space. While the majority of the subject lands are cleared, the north-western portion is grass-covered and there are a few trees located in the middle and north-western areas as well. A chain-link fence outlines the southern boundary, separating the subject lands from abutting properties at 6615 - 6633 Regional Road 20. The subject lands have a generally flat topography, however there is some minor descending sloping along the ditches, which traverse north to south in the southern and eastern portions of the subject lands

Please refer to **Figures 2-1 to 2-3** for an aerial view of the general location of the subject lands. **Figure 2-4** provides the topographical survey of the subject lands, which is also included separately.



**Figure 2-1:** Parcel Mapping of Subject Lands, Retrieved from Geowarehouse

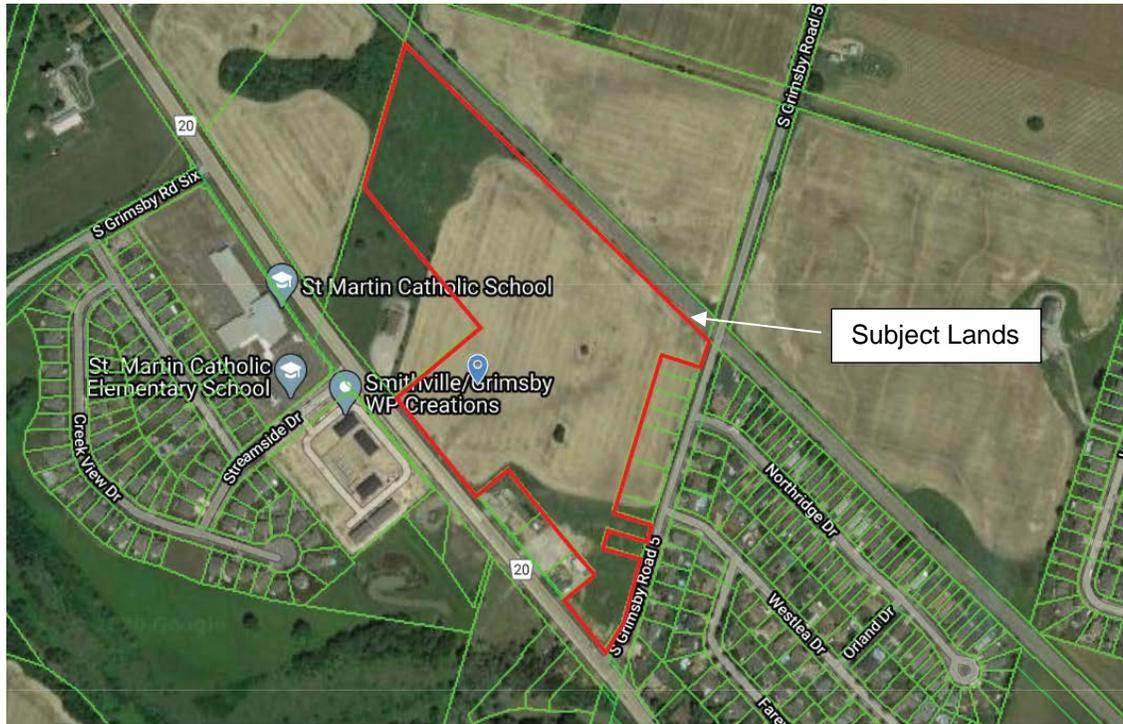


Figure 2-2: Aerial Imagery of the Subject Lands, Retrieved from Google Earth



Figure 2-3: Zoomed-in Aerial Image of the Subject Lands, Retrieved from Google Earth

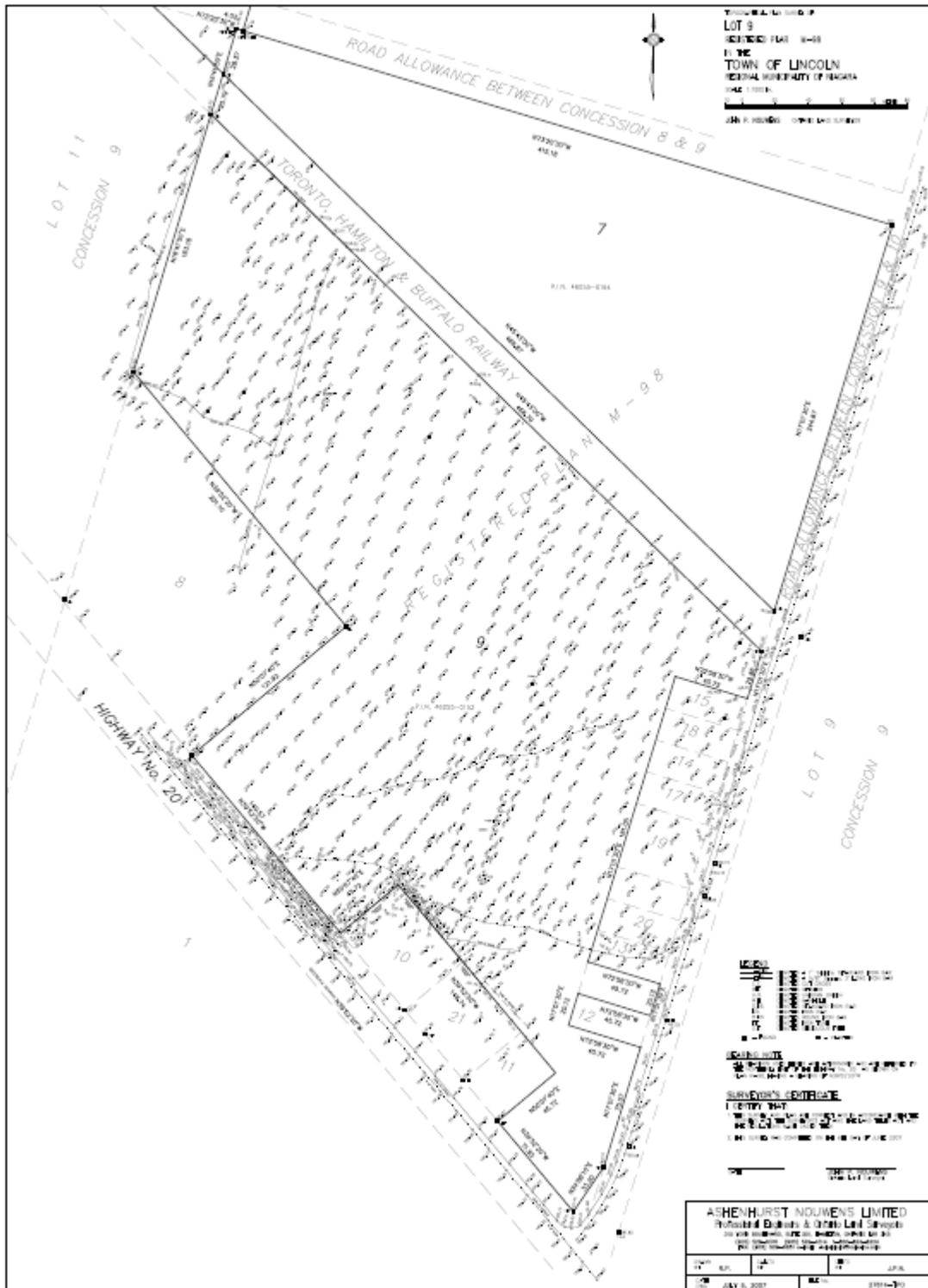


Figure 2-4: Topographical Survey of Subject Lands, prepared by Ashenhurst Nouwens Limited

## 2.2 Surrounding Context

In terms of surrounding context, the subject lands are located in the north-western portion of the Smithville Settlement Area, characterized by predominantly residential uses, as seen in **Figure 2-5**. A CP Rail Corridor is situated immediately adjacent to the north, comprising the entire northern boundary of the subject lands. On the opposite side of the CP Rail Corridor is an approved residential subdivision (“Dunloe”), which is proposed to consist of 29 single detached units, 20 semi-detached units, and 75 townhouse units, for a total of 124 residential units as well as a private storm-water management pond. The northern boundary of Dunloe outlines the limits of the Smithville Settlement Area. A dedicated right-of-way planned for a proposed collector road is situated to the north of the settlement area boundary and proposed residential subdivision. Further north, the lands are occupied by agricultural uses and non-farm related residential uses and are not included within the Settlement Area Boundary of Smithville.

Low density residential uses are located adjacent to the subject lands, just east of South Grimsby Road 5. Located further east is the West Lincoln Community Centre and Arena, which contains a library, arena complex, meeting rooms, and an outdoor skateboard park and playground. The area east of the West Lincoln Community Centre consists of predominately low density residential uses, and a few institutional uses (i.e. Saint Martin of Tours Roman Catholic Church).

A Rogers Cellular tower is located immediately to the south of the subject lands, fronting onto the north side of Regional Road 20. One single detached residential property is situated adjacent, directly to the west of the Cellular Tower. The area south of Regional Road 20, between the intersections of South Grimsby Road 5 and South Grimsby Road 6, is a residential subdivision, containing single detached and townhouse dwellings. St. Martin Catholic Elementary School is located at the northern edge of the subdivision, at the corner of Regional Road 20 and Streamside Road. The Streamside Pumping Station is situated adjacent to the townhouse dwellings, towards the east. Other low-density residential subdivisions exist to the south-east of Streamside Pumping Station. Twenty Mile Creek marks the limits of the Settlement Area Boundary and the lands further south of the creek are occupied by open space.

Directly to the west of the subject lands is a Greek Community Centre. Agricultural and non-farm related single detached residential uses are located further west of the subject lands.

A summary of the surrounding uses have been outlined below.

TO THE NORTH	TO THE EAST
<ul style="list-style-type: none"> <li>• CP Rail Corridor</li> <li>• Residential uses (Approved Subdivision containing 124 units)</li> <li>• Agricultural Uses</li> </ul>	<ul style="list-style-type: none"> <li>• Residential Uses</li> <li>• West Lincoln Community Centre and Arena</li> <li>• Institutional Uses (i.e. churches)</li> </ul>
TO THE SOUTH	TO THE WEST
<ul style="list-style-type: none"> <li>• Cellular Tower</li> <li>• Residential Uses</li> <li>• St. Martin Catholic Elementary School</li> <li>• Streamside Pumping Station</li> <li>• Open Space</li> </ul>	<ul style="list-style-type: none"> <li>• Greek Community Centre</li> <li>• Agricultural Uses</li> </ul>

**Table 2-1:** Surrounding Context

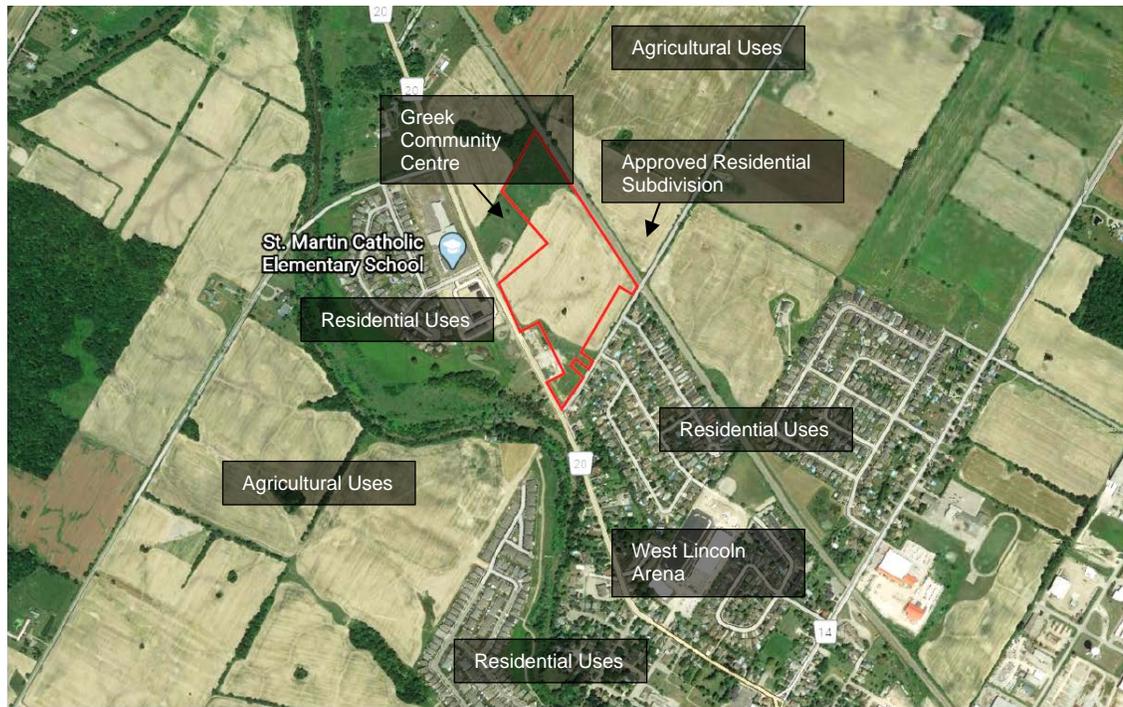


Figure 2-5: Aerial Imagery of the Surrounding Uses, Retrieved from Google Earth

### 3 Site and Area Images

Figures 3-1 to 3-8 below further show images of the existing conditions of the subject lands and surrounding context, retrieved from Google Street View and Google Earth.



Figure 3-1: 3-D North View of the Subject Lands and Surrounding Area, Retrieved from Google Earth



Figure 3-2: 3-D East View of the Subject Lands and Surrounding Area, Retrieved from Google Earth

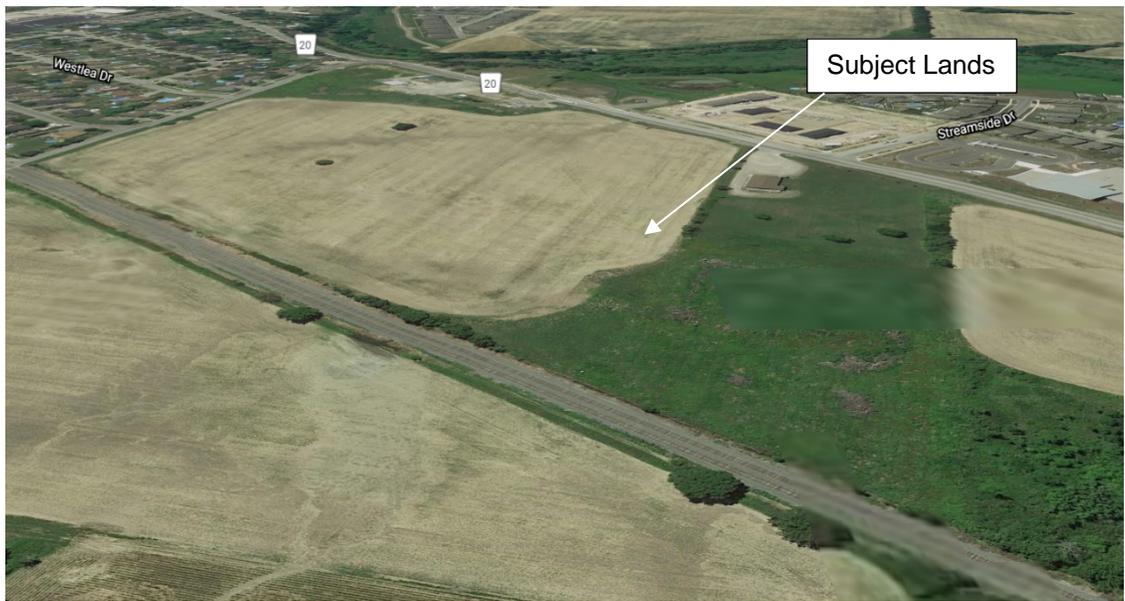


Figure 3-3: 3-D South Orientation of the Subject Lands, Retrieved from Google Earth



**Figure 3-4:** 3-D West View of the Subject Lands and Surrounding Area, Retrieved from Google Earth



**Figure 3-5:** View of the Subject Lands from the corner of the CP Rail Crossing and South Grimsby Road 5, Retrieved from Google Streetview



**Figure 3-6:** View of the Subject Lands from South Grimsby Road 5, Retrieved from Google Streetview



**Figure 3-7:** View of the Subject Lands from the intersection of South Grimsby Road 5 and Regional Road 20, Retrieved from Google Streetview



**Figure 3-8:** View of the Subject Lands from Regional Road 20, Retrieved from Google Earth

## 4 Development Proposal

The proposed development, 'Thrive' consists of one (1) three-storey mixed use residential commercial building at the corner of South Grimsby Road 5 and Regional Road 20, a storm-water management pond, open space and 224 residential units.

A storm-water management pond will be developed at the south-west corner of the subject lands. It will be dedicated to the Region, therefore maintained by the Region of Niagara or the Township of West Lincoln.

A four (4) metre wide trail, herein referred to as the 'Rail Trail' is proposed alongside the CP Rail Corridor, providing a buffer to the proposed residential units. Access to the 'Rail Trail' will be located midway and through a dedicated block which is located adjacent to the single detached dwellings in the north-east corner of the subdivision, to allow for pedestrian connectivity. Open space will be located at the entrance and terminus of the trail, in the north-eastern and north-western edges of the subject lands.

Portions of the proposed development have been designated as 'Future Development'. Preliminary concepts for these areas add an additional 26 single detached lots and 13 townhouse units. These areas will be finalized after adjacent parcels of land have been acquired to ensure that there are no issues relating to compatibility and orderly development of contiguous parcels.

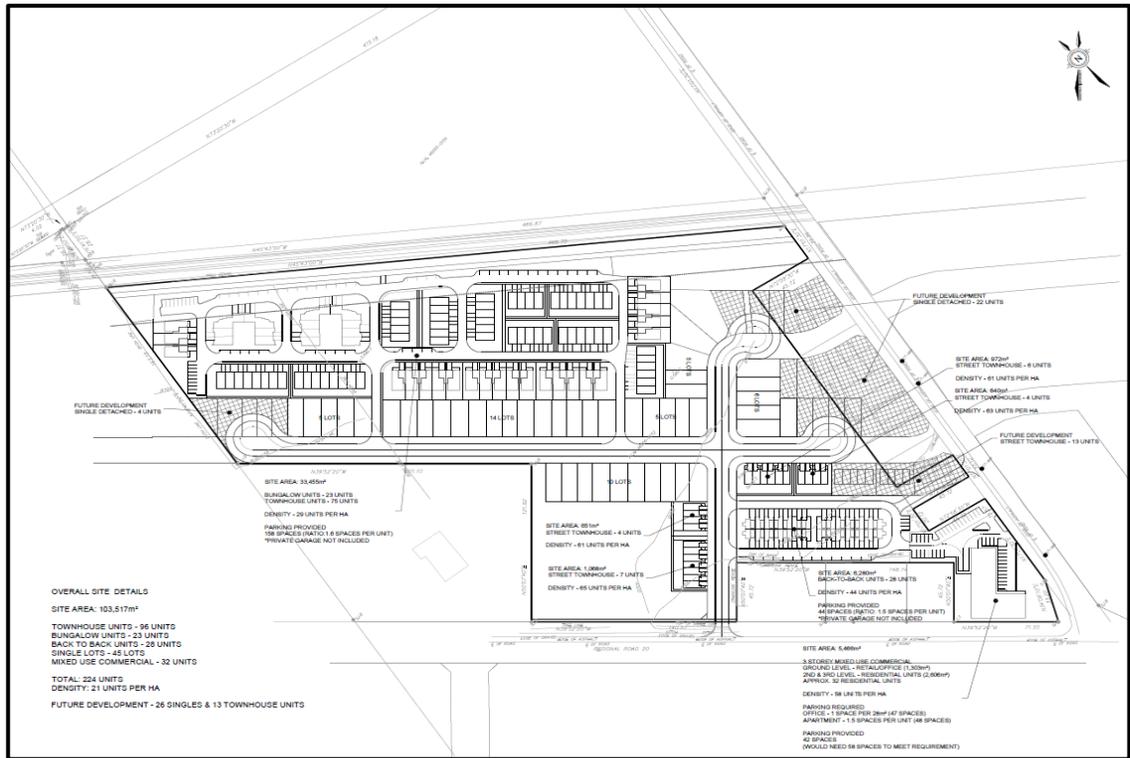
The residential units will have vehicular access to Regional Road 20, through a connection from a proposed municipal road, linked to another proposed municipal road and internal private roads. A temporary secondary entrance from South Grimsby Road 5, with removable bollards is intended for emergency access only. Two (2) future right-of-way accesses may be provided to the proposed development, depending on future acquisitions of properties along South Grimsby Road 5. Currently, these accesses terminate as cul-de-sacs and as such, the areas around the cul-de-sacs are designated as 'Future Development'.

The following table summarizes the composition of residential units in the proposed development by typology.

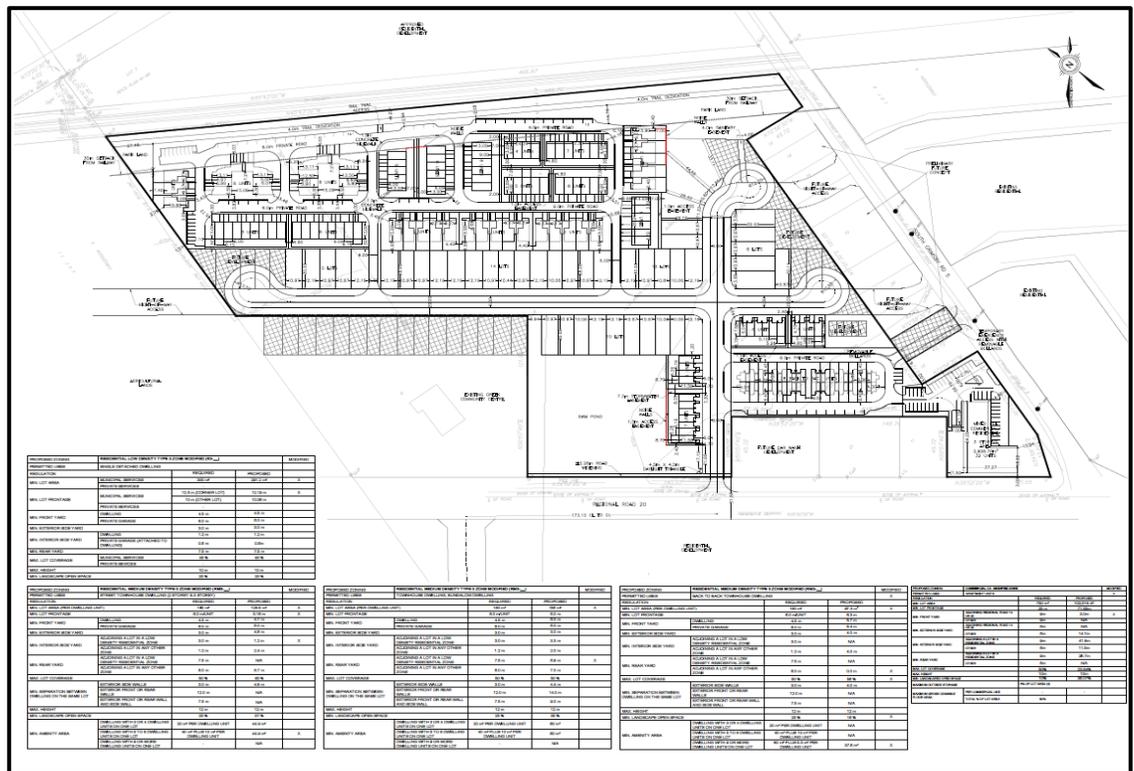
**Table 4-1:** Composition of Units by Dwelling Typology

UNIT TYPE	UNIT COUNT
2 storey Townhouse Units	75 units
Bungalow Townhouse Units	23 units
Back-to-Back Townhouse Units	28 units
3 Storey Townhouses	21 units
Single Detached Lots	45 lots
Mixed-use Residential	32 units (maximum)
<b>Total</b>	<b>224 units</b>

The preliminary concept, including the proposed uses for the future development areas is provided below in **Figure 4-1** and can also be found in **Appendix A**. The preliminary site plan, including the residential units, mixed-use building, open space and storm-water management pond is provided below in **Figure 4-2** and can also be found in **Appendix B**.

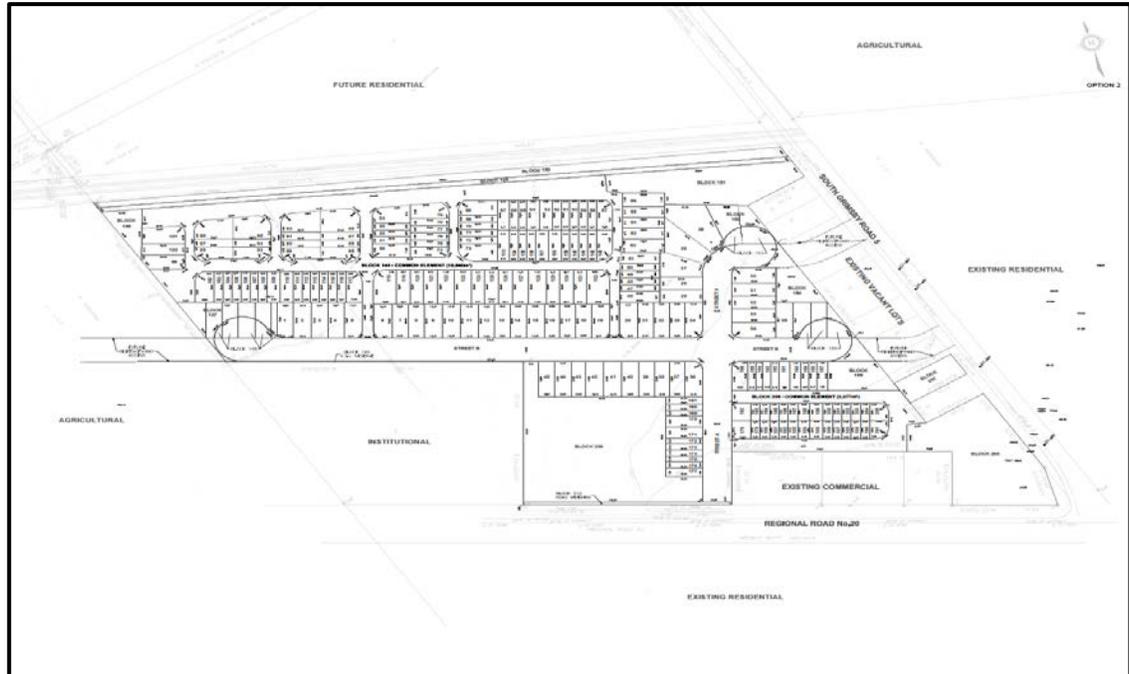


**Figure 4-1: Preliminary Concept Plan for the Proposed Development, prepared by IBI Group**

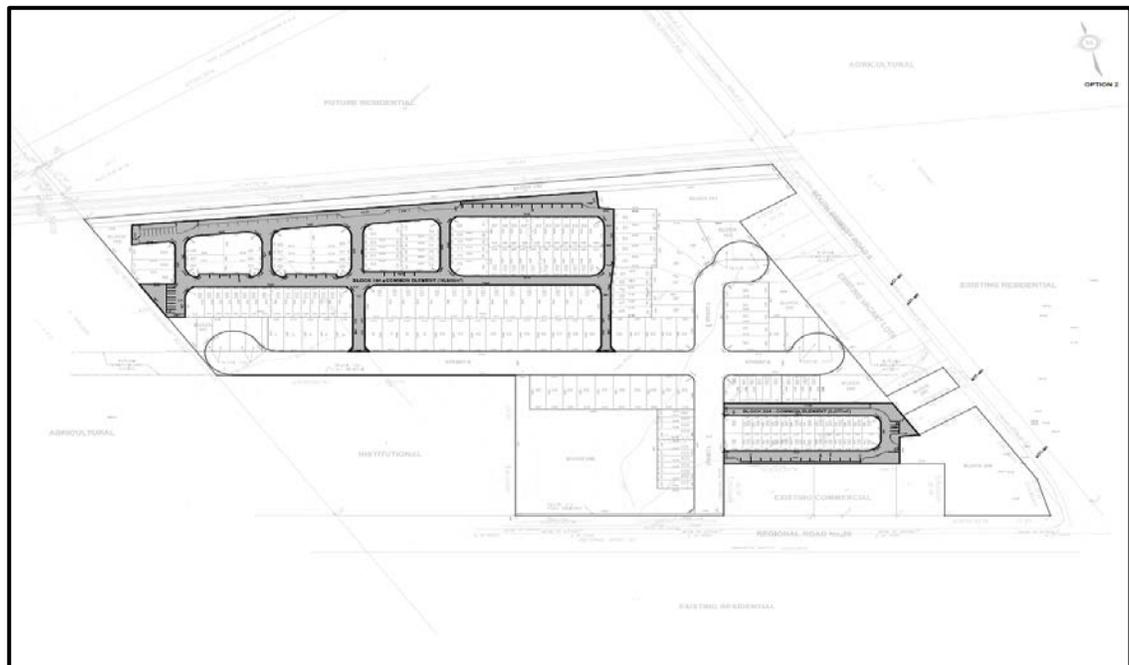


**Figure 4-2: Preliminary Site Plan Prepared by IBI Group**

The proposed draft plan of subdivision with lot and block details is found in **Figure 4-3** and **Appendix C**. The preliminary Draft Plan of Common Element Condominium is provided in **Figure 4-4** and **Appendix D**.

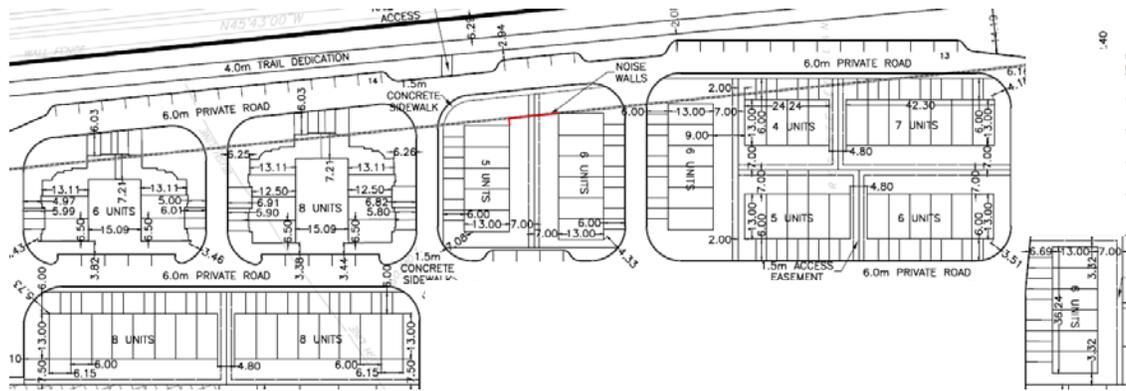


**Figure 4-3:** Preliminary Draft Plan of Subdivision, prepared by IBI Group



**Figure 4-4:** Preliminary Draft Plan of Common Element Condominium, prepared by IBI Group





**Figure 4-6:** Proposed Townhouse Units within Preliminary Site Plan, prepared by IBI Group

The table below provides the site statistics for the proposed townhouse units.

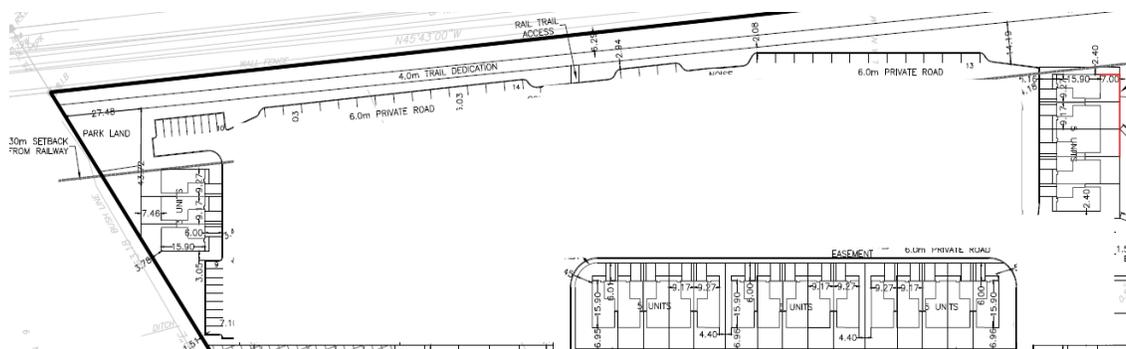
**Table 4-2:** Proposed Site Statistics for Townhouse Units

ITEM		PROPOSED
Number of Dwelling Units		75 units
Minimum Lot Area		128 square metres
Maximum Building Height		2 storeys
Minimum Front Yard		4.9 metres
Minimum Rear Yard		7.0 metres
Minimum Side Yard	Exterior	N/A
	Interior	4.8 metres
	Common Walls	0.0 metres

### 4.3 Proposed Bungalow Townhouse Units

A total of 23 bungalow townhouse units are proposed within the development, separated into four (4) blocks of five (5) units and one (1) block of three (3) units. The proposed bungalows will only be connected at the garage, and are otherwise separated. Each of the bungalow units will be one-storey in height, and have private rear yard amenity space.

Previously stated in the section above, visitor parking will be shared in this portion of the proposed development between two-storey townhouse and bungalow townhouse units.



**Figure 4-7:** Proposed Bungalow Townhouse Units within Preliminary Site Plan, prepared by IBI Group

**Table 4-3** provides the proposed site statistics for the bungalow townhouse units.

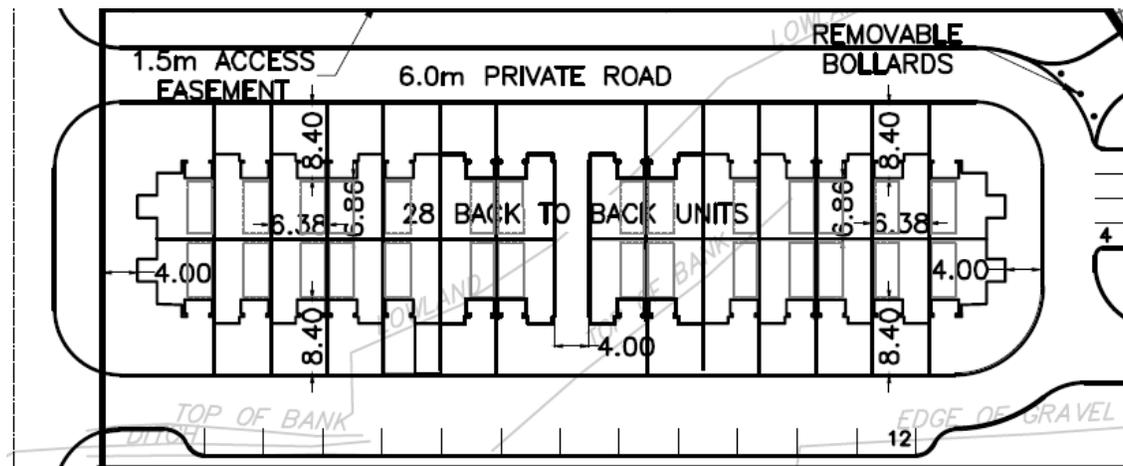
**Table 4-3:** Proposed Site Statistics for Bungalow Townhouse Units

ITEM		PROPOSED
Number of Dwelling Units		23 units
Minimum Lot Area		156 square metres
Minimum Lot Width		9.1 metres
Maximum Building Height		1 storey
Minimum Front Yard		6.0 metres
Minimum Rear Yard		6.9 metres
Minimum Side Yard	Exterior	N/A
	Interior	2.0 metres
	Common Walls	0.0 metres

#### 4.4 Proposed Back-to-Back Townhouse Units

The proposed development consists of 28 back-to-back townhouse units, separated into two (2) blocks located in the eastern portion of the subject lands, adjacent to the commercial block.

As seen in the figure below, visitor parking will be located to the south and east of the proposed back-to-back units, providing a total of 16 visitor parking spaces.



**Figure 4-8:** Proposed Back to Back Townhouse Units within the Preliminary Site Plan, prepared by IBI Group

The table below provides the site statistics for the proposed back-to-back units.

**Table 4-4:** Proposed Site Statistics for Back-to-Back units

ITEM	PROPOSED
Number of Dwelling Units	28 units

Minimum Lot Area		97 square metres
Minimum Lot Width		6.3 metres
Maximum Building Height		3 storeys
Minimum Front Yard		5.7 metres
Minimum Rear Yard		0.0 metres
Minimum Side Yard	Exterior	4.0 metres
	Interior	4.0 metres
	Common Walls	0.0 metres

### 4.5 Proposed Three-Storey Townhouse Units

There are 21 three-storey townhouse units within the proposed development, separated into one (1) block of seven (7) units, one (1) block of six (6) units and two (2) blocks of four (4) units. These dwelling units are located in the southern portion of the subject lands, in close proximity to the intersection of Regional Road 20 and the proposed municipal road. A 1.5 metre access easement will be provided for rear access to all of the proposed townhouse blocks. There is also a 7.3 metre overflow channel easement proposed between the southern-most blocks. The dwelling units will each have their own private rear yard amenity space.

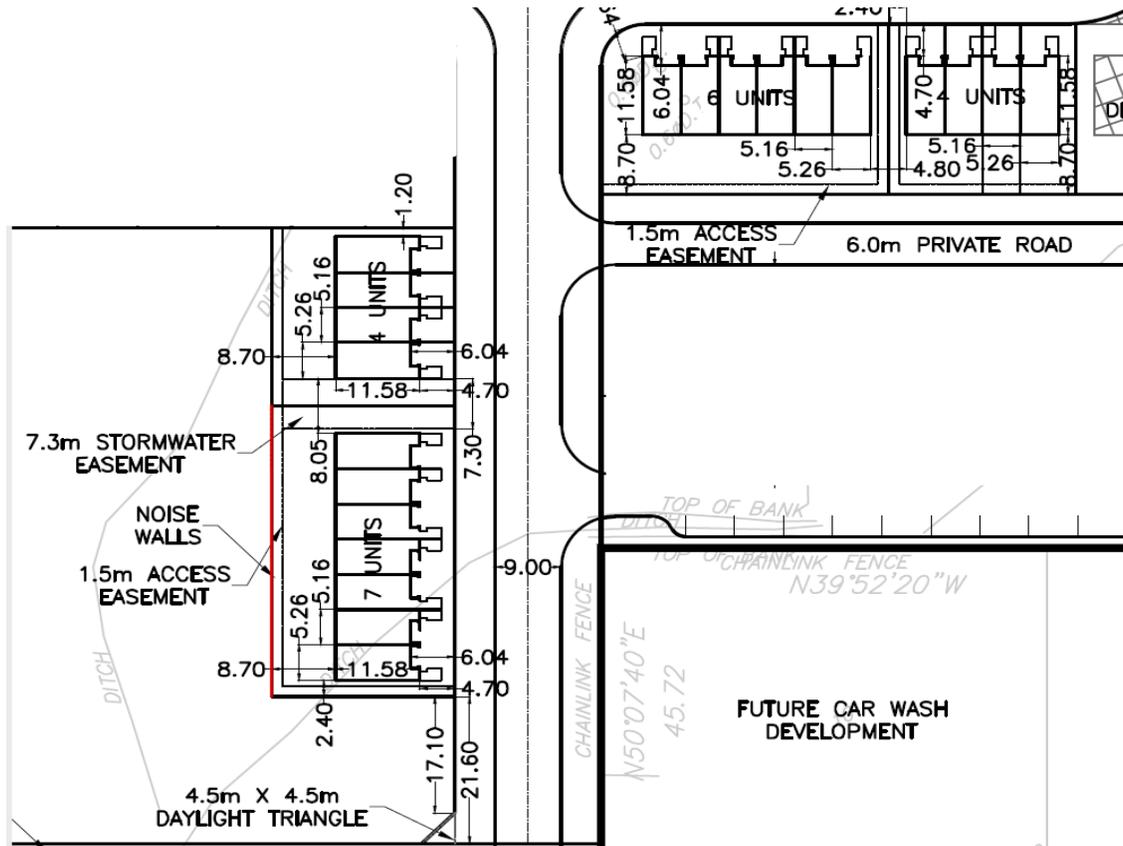


Figure 4-9: Proposed Three-Storey Townhouse Units within Preliminary Site Plan, prepared by IBI Group

Table 4-5 provides the site statistics for the proposed three-storey townhouse units.

**Table 4-5:** Proposed Site Statistics for Three-Storey Townhouse Units

ITEM		PROPOSED
Number of Dwelling Units		21 units
Minimum Lot Area		128 square metres
Maximum Building Height		3 storeys
Minimum Front Yard		4.7 metres
Minimum Rear Yard		8.7 metres
Minimum Side Yard	Exterior	4.6 metres
	Interior	4.7 metres
	Common Walls	0.0 metres

## 4.6 Proposed Single Detached Lots

There are 45 single detached lots proposed within the development, located in the central portion of the subject lands. These lots are divided into one (1) cluster of five (5) lots, one (1) cluster of six (6) lots, one (1) cluster of fourteen (14) lots, and two (2) clusters of ten (10) lots. The following table provides the site statistics for the proposed single detached lots.

**Table 4-6:** Proposed Site Statistics for Single Detached Lots

ITEM		PROPOSED
Number of Lots		45 units
Minimum Lot Area		291 square metres
Minimum Lot Frontage		10.0 metres
Maximum Building Height		10 metres
Minimum Front Yard		4.5 metres
Minimum Rear Yard		7.5 metres
Minimum Side Yard	Exterior	3.0 metres
	Interior	1.2 metres
	Common Walls	N/A

## 5 Formal Pre-Consultation

The purpose of a formal pre-consultation meeting is to review a proposed development, and identify the scope of information, studies and reports required to be submitted for a complete application. Pursuant to the requirements of the Township of West Lincoln, a pre-consultation meeting occurred on April 4, 2019 to discuss the subject lands, the concept plan prepared at the time, and the necessary planning approvals. The applicant/agent, owner, and various Region of Niagara and Township of West Lincoln staff were in attendance.

The concept plan prepared at the time of the meeting was for the development of approximately 201 residential units, including 52 single detached lots, 28 bungalow townhouse units, 28 back-to-back townhouses, and 76 standard townhouse units. The development was also proposed to have a storm-water management pond, open space and a commercial block located at the corner of Regional Road 20 and South Grimsby Road 5. The residential component of the proposed development has since been revised to a total of 224 residential units, including 45 single detached lots, 23 bungalow townhouse units, 28 back-to-back townhouses, 21 three-storey townhouse units, 75 two-storey townhouses and 32 mixed-use units.

Prior to the Formal Pre-Consultation Meeting, a preliminary planning review of the concept plan, dated December 7, 2018 was conducted by municipal planning and development staff. A summary of the initial planning comments are provided below. These comments are addressed by revisions to the concept plan, as well as planning comments in Section 8 of this report.

**Table 5-1:** Summary of Preliminary Planning Comments

AREA OF CONCERN	COMMENTS
Transportation	The proposed street pattern does not fully match with the approved street pattern on the secondary plan. The connections of Street G and F to South Grimsby Road 5, in alignment with Westlea Drive and Northridge Drive have been provided as well as Street H and Street G to South Grimsby Road 6. Street F does not extend the full length.
Trail Network	A connection to the trail midway through the development should be provided to allow for pedestrian access. Noise attenuation berms and fences, if transferred to the Township will be need to be designated with slopes that allow for regular maintenance.
Access	A secondary access to a public street should be provided. Roads that will temporarily dead end will require temporary cul-de-sacs until the remainder of the streets are constructed.
Land Use Designations	Township is supportive of the mix of single detached dwellings and townhouses.  The proposed low density residential zone is not exactly lined up with the low-density residential designation, and is slightly smaller than that shown on the secondary plan. The low-density area should be enlarged to match closer to the secondary plan.  The proposed medium density zone covers more area than what is shown in the secondary plan. Generally the layout of the units are acceptable, however more information is required (i.e. parking requirements, driveway widths, unit types, elevations).
Storm-water management	Storm-water management facilities should be integrated within the park to create a more usable public open space.
Park and Trail Linkage	A trail connection should also be included midway through the development, from Street G to the rail trail corridor.

The Formal Pre-Consultation Meeting confirmed that a Zoning By-law Amendment (“ZBLA”) and Draft Plan of Subdivision would be required. During the meeting, municipal staff indicated that a Local Official Plan Amendment would be required if the proposed development did not align with the North-west Quadrant Secondary Plan. This PJR will address how the proposed development

will align and meet the intent of Secondary Plan. In addition, staff also provided comments pertaining to; compatibility with adjacent properties, and the un-identified environmental features on the subject lands. As stated previously, further comments are addressed by planning comments in Section 8 of this report.

A summary of comments from various departments and agencies are provided in the table below.

**Table 5-2:** Summarized Department and Agency Comments

COMMENTING DEPARTMENT/ AGENCY	COMMENTS
Township of West Lincoln Planning	<p>Planning Justification will need to be provided for why the layout and designation of the secondary plan cannot be applied in this development.</p> <p>The proposed development does not show how the development corresponds to future development to the west or existing development to the east (road alignment of Westlea and Northridge).</p> <p>Urbanization will be required along the properties that front onto South Grimsby Road 5.</p> <p>The storm-water pond should be designed to incorporate lands to the north and meet design standards so that it is an attractive feature of the development, due to its prominent location.</p> <p>A full trail/park design shall be required as a condition of the subdivision.</p> <p>If the applicants are proposed to rezone the commercial designated area, the proposed zoning will be limited to those identified within the Secondary Plan. Access points should be shown for the Commercial Areas.</p>
Township of West Lincoln Public Works	Frontage of properties along South Grimsby Road 5 will need to be urbanized with sidewalks and lighting.
Township of West Lincoln Building	A Plan will be required to show where fill will be distributed on the site and how that affects building elevations.
Region of Niagara	<p>A constraints analysis is required as there are unmapped environmental features, including two water courses, treed areas in the north-west and south-east that exhibit wetland characteristics.</p> <p>A day-light triangle is required (4.5 x 4.5 metres) at the entrance along Regional Road 20.</p> <p>A Functional Servicing Report is required to identify servicing strategy, including the greater development area and identify if any upgrades are needed to the Streamside Pumping Station to accommodate the proposed development.</p> <p>A Storm-water management report will be required to show that runoff is captured and treated to an enhanced standard.</p>

Niagara Peninsula Conservation Authority	There is an area at the west-end of the subject lands that may contain an unevaluated wetland, therefore a constraints analysis is required to examine the feature.
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Alongside this Planning Justification Report, the following materials required by the Township of West Lincoln for a complete application have been submitted;

- Application form and fees
- Cover letter
- Conceptual Site Plan and Subdivision Plan
- Draft Zoning By-law Amendment
- Urban Design Report
- Archaeology Assessment
- Geotechnical Report
- Air Quality/Noise and Vibration Study
- Functional Servicing Study
- Stormwater Management Plan
- Transportation Impact Study/ Parking Impact Analysis
- Soil Report
- Gas Well Study
- Environmental Constraints Analysis

A copy of the Pre-Consultation Meeting Form is provided in **Appendix E**.

## 6 Supporting Studies, Reports and Plans

The Township's Pre-Consultation process outlined the information and materials required to submit a complete application. In accordance with this document, and in the interest of good planning, technical studies were completed. An overview of these professional studies and reports are provided below.

### 6.1 Urban Design Report

An Urban Design Report ("UDB") was prepared by Lindtack Architects Incorporated to address the proposed development against the applicable policy requirements of the various required Regional and Township Urban Design policies. The required policies include:

- Niagara Region Official Plan
- Niagara Region Model Urban Design Guidelines
- Township of West Lincoln Official Plan
- Smithville Trails and Corridor Master Plan
- Township of West Lincoln: Smithville Urban Design Manual

Key considerations for the overall development from this report are as follows:

- New roads are designed to allow for ease of traffic flow to each unit;
- Back-to-back units are located to the east of the main entrance off of Regional Road 20;
- Private road and visitor parking are located north of the site providing an effective buffer from the parallel train tracks;
- Sidewalks will ring the site to allow for pedestrian activity within the site and connect to municipal sidewalks which will promote the connection to downtown Smithville;
- Future landscape to be implemented which will promote effective shading and a visual barrier from traffic while providing aesthetically pleasing streetscape.

With regards to Building Design elements, key considerations from this report are as follows:

- private porches accessible from individual driveways
- Covered porches that reflect contemporary architecture
- Garage doors complement character and are recessed from the front façade
- Mix of materials (brick, stucco, stone, and metal) which will provide harmony
- Massing and rooflines and materials are varied
- Garages and driveways paired to reduce their visual impact
- Downward lighting at each entrance which will be recessed within the cover canopy

## 6.2 Archaeology Assessment

In April 2016, a Stage 1-2 Archaeological Assessment was prepared by Stantec to meet the required of the Provincial Policy Statement. The Archaeological Assessment was submitted to the Ministry of Tourism, Culture and Sport (MTCS) on May 2, 2016 in accordance with Part VI of the Ontario Heritage Act, R.S.O. c0.18. MTCS issued approval on May 2, 2016 and entered the report into the Ontario Public Register of Archaeological Reports without technical review.

Key conclusions from this assessment include:

- One pre-contact Aboriginal artifact was recovered on-site
- Due to the isolated nature of the artifact, the cultural heritage value of the subject lands was judged to be low.
- Therefore, the subject lands do not fulfill the criteria for a Stage 3 Archaeological Investigation.

## 6.3 Geotechnical/ Soils Report

A Geotechnical Investigation was completed by Soil-Mat Engineers and Consultants Ltd. ("Soil-Mat") in February 2020. The purpose of the Geotechnical Investigation was to determine soil and groundwater conditions. Key conclusions from this study include:

- The sampled material was found to exceed the standard for Electrical Conductivity, observed in areas near municipal roadways and parking lots resulting from the use of salt for winter de-icing operation. This is not harmful to human or animal life, but makes the soil environment unsupportive of plant growth and corrosive to buried grey or cast iron pipes.
- There is no evidence of potential impact (i.e. staining or odours)

## 6.4 Noise and Vibration Study

An Environmental Noise and Vibration Impact Study was completed by dBa Acoustical Consultants (“dBa”) in April 2020. The purpose of the study was to determine the noise impact from Regional Road 20 road traffic and CP Rail train traffic that may impact the proposed residential subdivision.

The main conclusion from the study was that the calculated noise levels exceed the 55 dBA criteria for outdoor amenity areas and the current STC calculations would not satisfy the criteria for interior noise reduction.

Key recommendations to mitigate noise control and satisfy the indoor and outdoor noise level criterion are as follows:

- Specific exterior doors
- Specific exterior wall construction
- Central Air Conditioning
- Provisions Central Air Conditioning
- Brick Veneer or Masonry Equivalent Wall Construction
- 2.5 metres Noise Barrier with Returns
- Specific Acoustically tested windows
- Registered warning clauses on title
- CP Rail warning classes on all units and blocks within 300 metres of the CP Rail Property Line
- Noise walls of 2.0 metres
- Window company to issue report that STC ratings are achieved

## 6.5 Functional Servicing Report

A Functional Servicing & Stormwater Management Report was prepared by AJ Clarke & Associates Inc. in May 2020. The purpose of this study was to provide how the site may be serviced by existing municipal sanitary, storm and water services. It also addresses stormwater management plan to assess quantity and quality in accordance with the current drainage and stormwater management policies and design criteria.

Key recommendations and conclusions concerning sanitary sewers, storm sewers, watermain, stormwater management, grading and roadworks are as follows:

- Road and lot grading has been established to minimize the cut and fill for the entire subdivision.
- Grading and roadworks will be carried out in accordance with the Town of West Lincoln engineering design standards.
- The proposed sanitary connection can be made to the existing Streamside Pumping Station. Upgrades will be required to the existing Pumping Station.
- An existing 200mm watermain on Regional Road 20 and a 150mm watermain on South Grimsby Road 5 can be utilized for servicing of the development.
- The proposed subdivision will drain into the proposed stormwater management facility and outflow to Twenty Mile Creek.

- An independent minor and major storm system has been designed and are to be routed to the stormwater management facility.
- The proposed Pond has been designed to capture minor and major storm flows. The Pond has been designed to provide quantity and quality control for the site.
- Erosion and Sediment control measures are proposed to ensure the amount of silt eroded during rainfall is kept to a minimum.

## 6.6 Transportation Impact Study/ Parking Impact Analysis

A Transportation Impact Study ("TIS") was prepared by IBI Group in February 2020. The purpose of the TIS was to analyze potential traffic impacts generated by the proposed development on the surrounding road network.

Key conclusions of the study include:

- Under 2020 existing conditions, the study area intersections operate within capacity and within acceptable levels of service during the weekday AM and PM peak hours
- Under 2025 future background conditions with the existing road network maintain, the study area intersections are expected to continue to operate within capacity and with acceptable levels of service during the AM and PM peak hours
- The proposed development is expected to generate a total of 117 and 180 net new trips during the weekday
- Transportation mode choice within the study area is expected to remain automobile dependent due to the absence of public transit service operating in the study area
- Under 2025 future total traffic conditions, traffic operations are expected to be comparable to 2025 future background and 2020 existing operations.
- Therefore, there are no recommendations necessary to improve traffic operations and increasing road traffic capacity.

## 6.7 Gas Well Study

A site walk of the property was completed by Angela Buonamici of IBI Group on Friday May 22<sup>nd</sup> 2020. As well, a site visit was completed by Ian Barret of Colville Consulting Inc to complete the spring field work. While both site visits concluded that there were no visual Gas Wells located on the site, we cannot confirm that any decommissioned gas wells are located on site.

If the Township wishes for further investigation regarding potential decommissioned Gas Wells, we request that this be requested through a condition of Draft Plan Approval.

## 6.8 Environmental Constraints Analysis

As soon as summer field work has been completed, an Environmental Constraints Analysis Report will be submitted by Colville Consulting Inc. A Spring Field Study site visit conducted by the consultant has concluded that there does not appear to be a lot of concern with this parcel. There is not much left in the way of vegetation on the site. As well, there is nothing on the property that would warrant amphibian work. While they are continuing to work through spring and summer field work, the consultant is not expecting to find anything that would require alteration to the development.

The property to the west of the subject property has a watercourse which is likely associated with a karst feature. There does not seem to be any similar concerns with this property.

Once the summer field work is complete, the final report will be finalized and submitted to the Town under separate cover.

## 7 Proposed Planning Applications

To permit the proposed development, approval of several planning applications will be required, including Draft Plan of Subdivision, Draft Plan of Common Element Condominium as well as a Zoning By-law Amendment (“ZBLA”).

### 7.1 Draft Plan of Subdivision

A Draft Plan of Subdivision will be required to obtain permission to subdivide the property. The proposed Draft Plan of Subdivision intends to subdivide the subject lands into 45 single detached lots, 98 two storey and bungalow townhouse lots, 21 three-storey townhouse lots and 28 back-to-back lots. It will also include 18 blocks and two new municipal right -of-ways (Streets A & B).

The proposed mixed-use residential commercial building will be located within Block 208, and will have a maximum of 32 residential units.

A storm-water management pond is proposed in the southern portion of the subject lands and will be located in Block 209, while parkland is situated in the northern portion of the subject lands, located in Blocks 148, 150 and 151. Blocks 146, 147, 152-156 and 207 have been designated as ‘Future Development’, subject to the acquisition of adjacent parcels. The majority of the Rail Trail is located within Block 149, while 0.3 metres of the Rail Trail will be located within Block 145. Block 210 includes the future road widening of Regional Road 20.

Blocks 144 and 206 will be a part of the Draft Plan of Common Element Condominium, further discussed in the following section.

The following table contains the details of the proposed Draft Plan of Subdivision.

**Table 7-1:** Proposed Draft Plan of Subdivision Details

BLOCK/LOT	PROPOSED USE	AREA (ha)	POTENTIAL NUMBER OF UNITS
Lots 1 – 45	Single Detached	1.61	45 units
Lots 46-143	Condominium Townhouse/Bungalow Units	2.28	98 units
Lots 157-177	Three-Storey Townhouse Units	0.33	21 units
Lots 178-205	Back-to-back Townhouse units	0.32	28 units
Block 144 & 206	Common Element – Private Road, Parking, Landscaping	1.37	N/A

Block 209	Storm-water Management Pond	0.95	N/A
Block 208	Commercial Block	0.55	32 units
Blocks 148,150 & 151	Parkland	0.71	N/A
Block 149	Trail Dedication	0.19	
Blocks 146, 147, 152 – 156 & 207	Future Development	0.64	N/A
Block 145	0.3 metre Reserve	0.0015	N/A
Block 210	Road Widening	0.032	N/A
Streets	Streets A & B	1.30	N/A

Please refer to **Appendix C** for the Draft Plan of Subdivision.

## 7.2 Draft Plan of Common Element Condominium

A Draft Plan of Common Element Condominium will be required to share ownership of a portion of the development, common to all owners. Common elements of the condominium will include landscaping and all internal private roads and parking, specifically around the proposed back-to-back and two-storey & bungalow townhouse units.

The following table contains the details of the proposed Draft Plan of Condominium.

**Table 7-2:** Proposed Draft Plan of Common Element Condominium Details

BLOCK	DESCRIPTION	AREA (ha)
Block 144	Common Element – private road, parking and landscaping	1.06
Block 206	Common Element – private road, parking and landscaping	0.31
<b>Total</b>		1.37

Please refer to **Appendix D** for the Draft Plan of Common Element Condominium.

## 7.3 Zoning By-law Amendment

The Township of West Lincoln Comprehensive Zoning By-law No. 2017-70 was passed by Council of the Township of West Lincoln on June 26, 2017. Zoning By-law No. 2017-70 replaces Zoning By-law 79-41 and implements the policies of the Township of West Lincoln Official Plan.

Currently, the subject lands are zoned 'Development' (D), which permits a limited range of uses, including conservation uses, public parks, and a single detached dwelling on an existing lot of record.

To accommodate the proposed development, a Zoning By-law Amendment ("ZBLA") will be required to rezone the subject lands into six different zones. To implement the proposed single detached lots, a ZBLA will be required to rezone this portion of the subject lands to the Residential Low Density Type 3 'R3' Zone, in order to allow for the dwelling typology. In regards to the R3

provisions, the proposed ZBLA will address site-specific provisions relating to the minimum lot area and lot frontage of corner lots.

To allow for the proposed two-storey, bungalow, back-to-back and three-storey townhouse units, a ZBLA will be required to rezone portions of the subject lands to the Residential Medium Density Type 3 'RM3' Zone, which permit street and standard townhouse dwellings. To implement the various townhouse dwelling typologies, the proposed ZBLA will include three different modified RM3 zones. With regards to the RM3 provisions for two and three storey townhouse units, the proposed ZBLA will address site-specific provisions pertaining to: minimum lot area, minimum lot frontage, minimum interior side yard, and minimum amenity area. For the bungalow townhouse units, the proposed ZBLA will address site-specific provisions relating to: minimum lot area per dwelling unit and minimum rear yard. In the case of back-to-back townhouse units, the proposed ZBLA will include the typology as a permitted use and address site-specific provisions pertaining to: minimum lot area per dwelling unit, minimum rear yard, maximum lot coverage, minimum landscaped open space and minimum amenity area.

The proposed commercial block will be rezoned to the Service Commercial 'C3', which permits a variety of commercial, retail and office uses. To allow for the proposed residential units on the second and third floors of the mixed-use building, the proposed ZBLA will add apartment residential units as a site-specific permitted use. It will also include site-specific provisions relating to the minimum front yard and reduced parking standards.

The proposed open space parkland, trail dedication and storm-water management pond will be appropriately rezoned to the Open Space 'OS' Zone.

Please refer to the proposed regulations in Section 9.1 of this report and the Draft Zoning By-law Amendment in **Appendix F**.

## 8 Current Planning Status

The subject lands were brought into the Urban Boundary of Smithville, through the Township's Official Plan Amendment 37, approved by the Region of Niagara Official Plan Amendment 3 with some modifications by the OMB. On June 26, 2017, the Township Council approved the Secondary Plan for the Northwest Quadrant, through Official Plan Amendment No.45.

The following subsections provide an assessment of the proposed development against current and applicable planning legislation and policy, including the Planning Act, Provincial Policy Statement 2014 & 2020, Growth Plan for the Greater Golden Horseshoe 2019, Region of Niagara Regional Official Plan, the Township of West Lincoln Official Plan, the Secondary Plan for the Northwest Quadrant, and the Township of West Lincoln Zoning By-law. These documents are reviewed in detail below and a number of planning comments are provided along with sub-section summaries.

### 8.1 Planning Act

The Planning Act, R.S.O. 1990, c. P.13 (the "Planning Act") deals with land use planning in Ontario and outlines how land is controlled and the roles of various levels of government and the public in regard to land use matters. **O.Reg 545/06** discusses the ability and requirements for a Municipality to enact and amend Zoning By-laws. Powers with respect to Zoning By-laws under Section 34 in the Planning Act include, amongst other things include: the ability to prohibit erecting, locating or using of buildings or structures; for prohibiting any use of land; for regulating the type of construction and the height, bulk, location size, floor area, spacing, character and use of buildings or structure; and for requiring provision and maintenance of parking facilities. The last

date of consolidation was December 10, 2019. The following excerpts and comments apply to the subject planning applications.

**Section 2** of the Planning Act refers to matters of **provincial interest** that the Minister, the council of a municipality, a local board, a planning board and the Tribunal, must have regard to, among other matters in order to carry out their responsibilities under the Act. Applicable matters listed in that section which pertain to the proposed applications include:

(a) *the protection of ecological systems, including natural areas, features and functions;*

**Planning Comment:** The Provincial Natural Heritage System Mapping identifies a significant woodland on the south-western and north-eastern portions of the subject lands, as depicted in **Figure 8-1**. The aerial imagery found in Section 3 of this report show that a small amount of vegetation remains. A preliminary site visit conducted by Colville confirm the previous statement, as there has been some alteration of the subject lands over the past few years. With little vegetation remaining, these areas do not function as a provincially significant woodland. Therefore, the proposed development will not negatively impact any natural features and functions.



**Figure 8-1:** Subject Lands within Natural Heritage Mapping, Retrieved from Provincial Natural Heritage Mapping

(d) *the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest;*

**Planning Comment:** An Archaeological Assessment was completed by Stantec, confirming that the cultural heritage or archaeological interest of the subject lands was judged to be low, therefore there are no concerns relating to archaeological interest.

(f) *the adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems;*

**Planning Comment:** The proposed storm-water management pond is planned based on the Northwest Quadrant Secondary Plan prepared by the Town of West Lincoln. As such, it will be dedicated to the Region of Niagara and maintained by either the Region or the Township. The stormwater management pond has been proposed at the lowest point of the subject lands and

sized to accommodate the proposed development as well as any areas immediately adjacent to the subject lands along Regional Road 20 and South Grimsby Road 5. The existing watermain along South Grimsby Road 5 between Dunloe and Thrive and the Streamside Pumping Station will be upgraded to accommodate the proposed development. The FSR prepared by AJ Clarke will discuss the efficient use of these facilities.

(h) *the orderly development of safe and healthy communities;*

**Planning Comment:** The subject lands are currently vacant, located within the Settlement Area of Smithville. The proposed development will contribute to attaining a compact built form in order to accommodate residential growth within the Township of West Lincoln. The proposed mixed-use residential commercial block will increase the mix of uses in the primarily residential area.

There are no safety concerns as a result of the land use compatibility and the location of the CP rail corridor on the northern edge of the subject lands. A four metre wide trail and adjoining park space will provide adequate separation for the proposed residential dwellings. Furthermore, the recommended mitigation measures within the Noise and Vibration Study, prepared by dBA Acoustical Consultants, will be implemented to mitigate any potential safety concerns.

(i) *the adequate provision and distribution of educational, health, social, cultural and recreational facilities;*

**Planning Comment:** There is an appropriate distribution of educational, health, social, and recreational facilities surrounding the subject lands to support the proposed increase in population. The subject lands are located within close proximity to institutional uses (i.e. St. Martin Catholic Elementary School) and the West Lincoln Community Centre and Arena. The proposed development will also provide approximately 0.7 hectares of parkland, located at the north eastern and north western portions of the subject lands.

(j) *the adequate provision of a full range of housing, including affordable housing;*

**Planning Comment:** Thrive will feature a mix and range of dwelling types, through the inclusion of townhouse (i.e. two-storey, three-storey, back-to-back & bungalow), apartment and single detached dwelling units. Since the majority of the residential units will be townhouse dwellings, this will contribute to attaining a compact built form, intended to be more affordable than a subdivision, comprised of primarily single detached dwellings.

(k) *the adequate provision of employment opportunities*

**Planning Comment:** A maximum of 1,303 square metres of ground floor commercial space is included within the proposed development. The mixed-use commercial block will provide additional employment opportunities, contributing to the range of employment uses in Smithville.

(o) *the protection of public health and safety;*

**Planning Comment:** Previously stated, there are no concerns relating to public safety due to the land use compatibility between the residential dwellings and the CP Rail Corridor. The proposed 'Rail Trail' and adjoining parkland, combined with the recommended mitigation measures in the Noise and Vibration Study will provide adequate buffering for the proposed residential dwellings.

(p) *the appropriate location of growth and development;*

**Planning Comment:** The subject lands are located within the Smithville Settlement Area boundary of the Town of West Lincoln and are designated as a 'Greenfield Area' within the Urban Structure of Smithville, as seen in **Figure 8-6**. Greenfield Areas are intended to accommodate new residential growth. As a result, the subject lands are an ideal and appropriate location for residential growth and development.

(r) *the promotion of built form that,*

(i) *is well-designed,*

(ii) *encourages a sense of place, and*

- (iii) *provides for public spaces that are of high quality, safe, accessible, attractive and vibrant;*

**Planning Comment:** The proposed development will be well-designed, creating an identifiable image at the entry point to the Smithville Settlement Area. It complies with the Town's vision for the Northwest Quadrant, identified in the Secondary Plan.

The proposed development will feature a mix of residential unit typologies and built form, which will create an interesting streetscape and encourage a sense of place. Through the inclusion of parkland, the proposed development will provide for a public space that is safe, accessible and vibrant.

**Section 3** of the Planning Act outlines that policy statements may be issued by the Minister. Subsection 5 requires that decisions on planning matters must be consistent with such Policy Statements.

**Planning Comment:** The following section of this report will confirm conformity of the proposed development with the Provincial Policy Statement.

**Section 51** of the Planning Act outlines the process for the Plan of Subdivision Approvals.

Subsection 24 provides the criteria for approving a Plan of Subdivision.

*In considering a draft plan of subdivision, regard shall be had, among other matters, to the health, safety, convenience, accessibility for persons with disabilities and welfare of the present and future inhabitants of the municipality and to*

- a) *the effect of development of the proposed subdivision on matters of provincial interest as referred to in section 2;*
- b) *whether the proposed subdivision is premature or in the public interest;*
- c) *whether the plan conforms to the official plan and adjacent plans of subdivision, if any;*
- d) *the suitability of the land for the purposes for which it is to be subdivided;*
- d.1) *if any affordable housing units are being proposed, the suitability of the proposed units for affordable housing;*
- e) *the number, width, location and proposed grades and elevations of highways, and the adequacy of them, and the highways linking the highways in the proposed subdivision with the established highway system in the vicinity and the adequacy of them;*
- f) *the dimensions and shapes of the proposed lots;*
- g) *the restrictions or proposed restrictions, if any, on the land proposed to be subdivided or the buildings and structures proposed to be erected on it and the restrictions, if any, on adjoining land;*
- h) *conservation of natural resources and flood control;*
- i) *the adequacy of utilities and municipal services;*
- j) *the adequacy of school sites;*
- k) *the area of land, if any, within the proposed subdivision that, exclusive of highways, is to be conveyed or dedicated for public purposes;*
- l) *the extent to which the plan's design optimizes the available supply, means of supplying, efficient use and conservation of energy; and*
- m) *the interrelationship between the design of the proposed plan of subdivision and site plan control matters relating to any development on the land, if the land is also located within a site plan control area designated under subsection 41 (2) of this Act or subsection 114*

(2) of the City of Toronto Act, 2006. 1994, c. 23, s. 30; 2001, c. 32, s. 31 (2); 2006, c. 23, s. 22 (3, 4); 2016, c. 25, Sched. 4, s. 8 (2).

**Planning Comment:** All of the matters above will be addressed through **Section 8** of this report.

**Planning Act Conclusion:** The proposed development is subject to Section 34 and 51 of the Planning Act, as amendments are required to the Town of West Lincoln Zoning By-law and a Draft Plan of Subdivision will be submitted concurrently. The applications will be processed in accordance of those sections of the Act.

The proposed development is consistent with the Planning Act, as it:

- Does not develop upon provincially significant woodlands and vegetation
- utilizes municipal infrastructure to accommodate the proposed development
- Increases the mix and range of housing options for the Town of West Lincoln, through the inclusion of townhouse (i.e. two-storey, three-storey, bungalow and back to back), apartment and single detached dwellings
- Provides employment opportunities through the direct inclusion of ground floor commercial space
- Protects public safety from the CP Rail Corridor, through the separation from residential dwellings by parkland and the implementation of mitigation measures recommended within the Noise and Vibration Study
- Represents development on 'Greenfield' designated lands, and therefore, is an appropriate location for residential growth

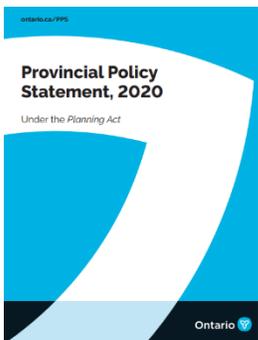
## 8.2 Provincial Policy Statement 2020

The Provincial Policy Statement 2020 ("PPS") provides policy direction on matters of provincial interest regarding land use planning and sets the foundation for land use planning and development regulations. Main considerations of this document pertain to protecting resources of provincial interest, the built and natural environment and public health and safety. The PPS focuses growth within Settlement Areas and away from significant or sensitive resources and areas which may pose a risk to public health and safety. It recognizes that the wise management of development may involve directing, promoting or sustaining growth. Land use must be carefully managed to accommodate appropriate development to meet the full range of current and future needs, while achieving efficient development patterns.

On February 28, 2019, the Provincial Government issued a new Provincial Policy Statement 2020. It replaced the Provincial Policy Statement issued April 30, 2014. The PPS was issued under Section 3 of the Planning Act and came into effect on May 1, 2020. In this regard, Section 3 of the Planning Act requires that land use planning decisions be consistent with the PPS. The PPS provides direction for municipal planning documents and to individual site-specific developments. Municipal Official Plans are to be consistent with the PPS. The PPS applies to all decisions in respect of the exercise of any authority that affects a planning matter made on or after this date. Therefore, the proposed applications are subject to and is assessed against the applicable policies of the PPS.

For the purposes of this report, the version of the PPS available online at <https://files.ontario.ca/mmah-provincial-policy-statement-2020-accessible-final-en-2020-02-14.pdf> was used.

The PPS is to be read in its entirety. With respect to the specific policies in the PPS 2020 document, several are applicable to the subject lands in general, and to the proposed development, and Zoning By-law Amendment specifically.



## 8.2.1 Section 1 Building Strong, Healthy Communities

**Section 1** of the PPS focuses on building strong, healthy communities. **Sub-Section 1.1** provides direction for managing and directing land use to achieve efficient and resilient development and land use patterns.

**Policy 1.1.1** *Healthy, liveable and safe communities are sustained by:*

- a. *promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*

**Planning Comment:** The proposed development will allow for the development of 224 residential dwellings in a location that has considerable development potential within the Urban Area of Smithville in the Township of West Lincoln. It will develop the subject lands, which are part of the Township's Designated Greenfield Area ("DGA"), and will be compatible with existing residential built form in the surrounding neighbourhood to the east and south. The proposed development will efficiently utilize the subject lands and municipal infrastructure to accommodate anticipated residential and employment growth. This will reduce the need for uneconomical land expansions, thereby sustaining the financial wellbeing of the Province. Given that the proposed development will provide residential density on a parcel, designated as 'Greenfield Area' ("DGA") and utilizes municipal infrastructure, it is our opinion that the proposed development is a form of 'efficient' development.

- b. *accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*

**Planning Comment:** The proposed development, Thrive will contribute to the existing housing stock in the Settlement Area of Smithville, by providing an appropriate range and mix of dwelling typologies on the subject lands, including single detached, apartment dwelling units and various townhouse dwellings. The commercial component will add to the range and mix of uses within the neighbourhood. The proposed retail/office uses and open space will service the long term needs of residents, in the proposed residential subdivision, and surrounding neighbourhood. As seen in the Draft Plan of Subdivision, the site layout and design will provide dwelling type diversity, whilst being compatible and responsive to the mixture of built form on the subject lands.

- c. *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*

**Planning Comment:** Confirmed by Colville Consulting, the subject lands contain little vegetation, therefore the proposed development will not cause any environmental concerns. This will be further explained in the full Environmental Constraints Analysis Report which will be provided under separate cover once the summer field work is completed.

The northern portion of the subject lands is abutted by a CP Rail Corridor. The proposed development will avoid a land use pattern that may cause public health and safety concerns, by separating the residential dwellings and the rail corridor with parkland and open space.

- e. *promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;*
- g. *ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs; and*

**Planning Comment:** The proposed development represents a cost effective development pattern, as it will provide a compact built form on lands designated for residential growth. The

existing watermain along South Grimsby Road 5 between Dunloe and Thrive as well as the Streamside Pumping Station will be upgraded to accommodate the proposed development, therefore it will make efficient utilization of municipal infrastructure and minimize the expansion of municipal services. The FSR prepared by AJ Clarke will show that the necessary infrastructure will be able to meet the current and projected needs of the proposed development.

**Policy 1.1.2** *Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 20 years. However, where an alternate time period has been established for specific areas of the Province as a result of a provincial planning exercise or a provincial plan, that time frame may be used for municipalities within the area.*

*Within settlement areas, sufficient land shall be made available through intensification and redevelopment and, if necessary, designated growth areas.*

**Planning Comment:** As noted previously, the subject lands were brought into the Urban Boundary of Smithville, through the Township's Official Plan Amendment 37 in 2015. By approving the expansion of the Settlement Area Boundary, this indicates that the Township of West Lincoln intends for the subject lands to accommodate projected growth.

**Subsection 1.1.3** provides development and growth direction for settlement areas.

**Policy 1.1.3.1** *Settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted.*

**Policy 1.1.3.2** *Land use patterns within settlement areas shall be based on densities and a mix of land uses which:*

- a. *efficiently use land and resources;*
- b. *are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c. *minimize negative impacts to air quality and climate change, and promote energy efficiency*
- d. *prepare for the impacts of a changing climate*
- e. *support active transportation;*
- f. *are transit-supportive, where transit is planned, exists or may be developed; and*

*Land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated*

**Policy 1.1.3.3** *Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.*

**Policy 1.1.3.4** *Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.*

**Policy 1.1.3.6** *New development taking place in designated growth areas should occur adjacent to the existing built-up area and shall have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.*

**Planning Comment:** As the subject lands are located within the Urban Settlement Area of the Township of West Lincoln, the proposed development will provide growth and development in an appropriate location. In addition, the subject lands are also identified as a part of the Town's Designated Greenfield Area ("DGA"), and thus will contribute to planning for and achieving sufficient density to ensure efficient growth and development in accordance with in-effect targets at the provincial and regional level. The proposed development will efficiently utilize the overall area of the subject lands, by providing a compact, functional, and compatible layout of residential and commercial development on DGA lands. It is appropriate and will efficiently utilize municipal infrastructure. Currently, the Town of West Lincoln does not have a transit system.

**Subsection 1.2.6** provides policies in relation to land use compatibility.

**Policy 1.2.6.1** *Major facilities and sensitive land uses shall be planned and developed to avoid, or if avoidance is not possible, minimize and mitigate any potential adverse effects from odour, noise and other contaminants, minimize risk to public health and safety, and to ensure the long-term operational and economic viability of major facilities in accordance with provincial guidelines, standards and procedures.*

**Planning Comment:** There are no sources of odour or contamination near the subject lands. The proposed residential dwellings will be appropriately separated from the CP Rail Corridor to avoid any adverse noise effects and minimize the risk to public safety, through the inclusion of park space and the orientation of dwellings, placing private amenity space at the rear. The recommended measures within the Noise and Vibration Study, prepared by dBA Acoustical Consultants will be implemented to mitigate adverse noise effects.

**Subsection 1.3** provides policies relating to employment.

**Policy 1.3.1** *Planning authorities shall promote economic development and competitiveness by:*

- a. *providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;*
- b. *providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses*
- d. *encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities, with consideration of housing policy 1.4; and*

**Planning Comment:** Thrive will include a compact mixed-use residential commercial building at the corner of South Grimsby Road 5 and Regional Road 20. A maximum of 1,303 square metres of retail/office space is proposed on the ground floor. These uses will be compatible with the surrounding neighbourhood and support the anticipated residential growth and existing residential uses in the community. The inclusion of office/retail space will contribute to the mix and range of employment uses within the Settlement Area of Smithville and the Town of West Lincoln, to help meet the employment target of 265,000 jobs identified for the Region of Niagara in the Greater Golden Horseshoe Plan 2019. According to the Township of West Lincoln Economic Development Findings and Issues Report in 2016, anecdotal evidence suggests that employment growth is

lagging, as growth in the agricultural and manufacturing sectors is low. The inclusion of commercial space will provide opportunities to diversify the economic base of the municipality.

**Subsection 1.4** provides direction to guide residential housing development.

**Policy 1.4.1** *To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:*

- a. *maintain at all times the ability to accommodate residential growth for a minimum of 10 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and*
- b. *maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.*

**Policy 1.4.2** *Where planning is conducted by an upper-tier municipality:*

- a. *the land and unit supply maintained by the lower-tier municipality identified in policy 1.4.1 shall be based on and reflect the allocation of population and units by the upper-tier municipality; and*
- b. *the allocation of population and units by the upper-tier municipality shall be based on and reflect provincial plans where these exist.*

**Policy 1.4.3** *Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents of the regional market area by:*

- b. *permitting and facilitating:*
  1. *all forms of housing required to meet the social, health and well-being requirements of current and future residents, including special needs requirements; and*
  2. *all forms of residential intensification, including second units, and redevelopment in accordance with policy 1.1.3.3;*
- c. *directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*
- d. *promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed; and*
- f. *establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety*

**Planning Comment:** The Town of West Lincoln designated the subject lands as part of the Greenfield Area, available to accommodate projected residential growth for a minimum of 10 years. The proposed development will contribute to accommodating anticipated residential growth and housing needs of a diverse population, through providing an appropriate range and mix of housing types and densities.

As a two-tier municipality, the Region of Niagara has allocated approximately 3% of the total household and population growth to the Town of West Lincoln, forecasting a total household of

5,610 and a population of 16,990 by 2031 identified in **Figure 8-3**. The proposed development will provide approximately 224 residential units, which will help contribute in accommodating the noted population and household growth as well as diversify the overall housing stock within the neighbourhood. The proposed layout will make an efficient use of the subject lands, through the regular arrangement of townhouse blocks and the use of internal condominium roadways, allowing for a compact form of development.

The proposed dwellings will ultimately connect to and utilize existing municipal infrastructure, as well as public service facilities, confirmed by the FSR prepared by AJ Clarke.

**Subsection 1.5** provides policies for public spaces, recreation, parks, trails and open space.

**Policy 1.5.1** *Healthy, active communities should be promoted by:*

- b. *planning and providing for a full range and equitable distribution of publicly-accessible built and natural settings for recreation, including facilities, parklands, public spaces, open space areas, trails and linkages, and, where practical, water-based resources;*

**Planning Comment:** The inclusion of the 'Rail Trail' and adjoining parkland will add to the range and distribution of publicly-accessible natural settings within the edge of the Smithville Urban Area to service the anticipated population growth.

**Subsection 1.6.6** provides policies relating to sewage, water and storm-water.

**Policy 1.6.6.1** *Planning for sewage and water services shall:*

- a. *direct and accommodate expected growth or development in a manner that promotes the efficient use and optimization of existing:*
  - 1. *municipal sewage services and municipal water services; and*
- b. *ensure that these systems are provided in a manner that:*
  - 1. *can be sustained by the water resources upon which such services rely;*
  - 2. *is feasible, financially viable and complies with all regulatory requirements; and*
  - 3. *protects human health and the natural environment;*
- d. *integrate servicing and land use considerations at all stages of the planning process; and*

**Policy 1.6.6.2** *Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services*

**Planning Comment:** The FSR, prepared by AJ Clarke provided that the proposed development will optimize available municipal sewage and water services. Upgrades will be required to the watermain along South Grimsby Road 5 and the Streamside pumping station. Please refer to the FSR for details regarding final engineering designs.

**Policy 1.6.6.7** *Planning for stormwater management shall:*

- a. *minimize, or, where possible, prevent increases in contaminant loads;*
- b. *minimize changes in water balance and erosion;*
- c. *not increase risks to human health and safety and property damage;*

- d. *maximize the extent and function of vegetative and pervious surfaces; and*
- e. *promote stormwater management best practices, including stormwater attenuation and re-use, and low impact development*

**Planning Comment:** The proposed storm-water management pond has been planned in accordance with the Northwest Quadrant Secondary Plan prepared by the Town of West Lincoln and will be dedicated to the Region of Niagara. It has been sized to accommodate the proposed development.

**Subsection 1.6.8** provides policies in relation to Transportation and Infrastructure Corridors.

**Policy 1.6.8.2** *Major goods movement facilities and corridors* shall be protected for the long term.

**Policy 1.6.8.3** *Planning authorities shall not permit development in planned corridors that could preclude or negatively affect the use of the corridor for the purpose(s) for which it was identified.*

*New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities.*

**Planning Comment:** The subject lands abut a CP Rail Corridor to the north. The proposed development will be compatible with the rail corridor and not negatively affect the use and purpose of the transportation facility, through adequate buffering by parkland and dwelling orientation, placing amenity space to the rear. The amenity space will be further compatible with the rail corridor, through the implementation of a noise wall as recommended by the Noise Study prepared by dBa Acoustical Consultants.

Section 1.7 provides policies pertaining to Long-Term Economic Prosperity.

**Policy 1.7.1** *Long-term economic prosperity should be supported by:*

- a. *promoting opportunities for economic development and community investment-readiness;*
- b. *encouraging residential uses to respond to dynamic market-based needs and provide necessary housing supply and range of housing options for a diverse workforce;*
- c. *optimizing the long-term availability and use of land, resources, infrastructure and public service facilities;*

**Planning Comment:** The subject lands are located within an area where existing municipal services are available along South Grimsby Road 5 and at the Streamside Pumping Station to accommodate the proposed commercial space. The proposed development will support long-term economic prosperity and diversify the Town's economy, by providing a maximum of 1,303 square metres of commercial space within a well-designed mixed-use residential building.

The diversity of dwelling typologies, resulting in a range of housing options including single detached, townhouse and apartment dwelling units will respond to a variety of market based needs for a diverse workforce.

## **8.2.2 Section 2 Wise Use and Management of Resources**

**Section 2.0** of the PPS provides policies pertaining to the **Wise Use and Management of Resources**. **Subsection 2.1** provides policies relating to the protection of Natural Heritage.

**Policy 2.1.1** *Natural features and areas shall be protected for the long term.*

**Policy 2.1.5** *Development and site alteration shall not be permitted in:*

- b. *significant woodlands* in Ecoregions 6E and 7E (excluding islands in Lake Huron and the St. Marys River)<sup>1</sup>

*unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions.*

**Planning Comment:** As seen in **Figure 8-1**, portions of the subject lands have been identified as a significant woodland. A preliminary site visit conducted by Colville Consulting identify that a small amount of vegetation remains as there has been some alteration of the subject lands over the past few years. With little vegetation on the site as mentioned in the previous section regarding Supporting Studies, these areas do not function as a provincially significant woodland.

**Provincial Policy Statement 2020 Conclusion:** The proposed development is consistent with the Provincial Policy Statement, as;

- the layout and inclusion of townhouse blocks more efficiently utilizes the vacant lands, than a single detached residential subdivision
- the subject lands are a designated 'Greenfield Area' where residential growth and development shall be focused
- the dwelling type diversity will contribute to the existing housing stock within the Smithville Settlement Area, necessary to promote the long term liveability and well-being of the West Lincoln Community
- promotes economic development and fosters long-term economic prosperity, through providing a maximum of 1,303 square metres of commercial space
- can be fully serviced by municipal infrastructure along South Grimsby Road 5 and the Streamside Pumping Station
- the inclusion of the park space will reduce public safety hazards from the CP Rail Corridor

### 8.3 Growth Plan for the Greater Golden Horseshoe 2019

The Growth Plan for the Greater Golden Horseshoe (the "Growth Plan") 2019 was prepared and approved under the Places to Grow Act, 2005. The Growth Plan took effect on May 16, 2019 and is applicable to the subject lands. The Growth Plan provides policies to guide future growth.

The Growth Plan provides policies to guide future growth and development, where the major goals are to provide a sufficient housing supply, improving transportation options, encourage a high quality of life and a strong economy, while ensuring a healthy natural environment. The Growth Plan guides development in the Greater Golden Horseshoe ("GGH") to a time horizon to the year 2041. Overall, the Growth Plan has projected a 2041 population of 610,000 and employment of 265,000 for the Region of Niagara. The Region of Niagara is currently undergoing a Regional Official Plan Review in order to plan for the forecasted growth.

For the purposes of this report, the version of the Growth Plan available online at <https://www.ontario.ca/document/place-grow-growth-plan-greater-golden-horseshoe> was used.

The following policies discussed are particularly applicable to the development proposal.



### 8.3.1 Section 1 Introduction

**Section 1.2.1** provides the guiding principles of the Greater Golden Horseshoe. Relevant principles include;

- *Support the achievement of complete communities that are designed to support healthy and active living and meet people's needs for daily living throughout an entire lifetime.*

**Planning Comment:** The proposed development will provide residential growth and commercial space, efficiently utilizing the subject lands and existing infrastructure to support complete community and meet people's needs for daily living.

- *Support a range and mix of housing options, including second units and affordable housing, to serve all sizes, incomes, and ages of households.*

**Planning Comment:** The proposed development will increase the range and mix of housing options, through the introduction of two and three storey townhouses, bungalow townhouses, back-to-back townhouses, single detached and apartment units on the subject lands. This will diversify the housing stock at the edge of Smithville, while being compatible with neighbourhoods to west and south.

- *Protect and enhance natural heritage, hydrologic, and landform systems, features, and functions.*

**Planning Comment:** There are no concerns relating to the protection of natural heritage features, as the existing vegetation is minimal and does not function as a provincially significant woodland.

### 8.3.2 Section 2 Where and How to Grow

**Section 2.2.1** of the Growth Plan deals with managing growth, and the following policies within it are applicable to the proposed development

**Policy 2.2.1.1** Population and employment forecasts contained in Schedule 3 will be used for planning and managing growth in the GGH to the horizon of this Plan in accordance with the policies in subsection 5.2.4.

- a. *the vast majority of growth will be directed to settlement areas that:*
  - i. *have a delineated built boundary;*
  - ii. *have existing or planned municipal water and wastewater systems; and*
  - iii. *can support the achievement of complete communities;*
- c. *within settlement areas, growth will be focused in:*
  - i. *delineated built-up areas;*
  - ii. *strategic growth areas;*
  - iii. *locations with existing or planned transit, with a priority on higher order transit where it exists or is planned; and*
  - iv. *areas with existing or planned public service facilities*
- d. *development will be directed to settlement areas, except where the policies of this Plan permit otherwise;*
- f. *the establishment of new settlement areas is prohibited.*

**Planning Comment:** The subject lands are within the current Urban Area of Smithville, and are located within a 'DGA', a strategic growth area designated and planned for residential growth and development.

The proposed development will efficiently utilize existing municipal water and wastewater systems to support 224 residential units, by developing a planned storm-water management pond and upgrading the Streamside Pumping Station and the watermain along South Grimsby Road 5. It will support the achievement of complete communities, by providing a mix of residential and commercial uses, as well as residential dwelling types and diversities.

**Policy 2.2.1.4** *Applying the policies of this Plan will support the achievement of complete communities that:*

- a. *feature a diverse mix of land uses, including residential and employment uses, and convenient access to local stores, services, and public service facilities;*

**Planning Comment:** The proposed development, consisting of residential uses and commercial space will increase the diversity of land uses and employment opportunities within the Town of West Lincoln. Placing the proposed mixed-use residential commercial building at a prime intersection will provide future and existing residents convenient access to local stores.

- c. *provide a diverse range and mix of housing options, including second units and affordable housing, to accommodate people at all stages of life, and to accommodate the needs of all household sizes and incomes;*
- e. *provide for a more compact built form and a vibrant public realm, including public open spaces;*

**Planning Comment:** The proposed development, consisting of townhouses, bungalow townhouses, back-to-back townhouses, apartment dwelling units and single detached units will increase the mix of residential types and availability of housing options in the Settlement Area of Smithville. The layout of the proposed development will provide for a more compact built form, while including public open spaces to service the anticipated population growth.

**Section 2.2.5** of the Growth Plan provides policies relating to Employment.

**Policy 2.2.5.1** *Economic development and competitiveness in the GGH will be promoted by:*

- b. *ensuring the availability of sufficient land, in appropriate locations, for a variety of employment to accommodate forecasted employment growth to the horizon of this Plan;*

**Policy 2.2.5.15** *The retail sector will be supported by promoting compact built form and intensification of retail and service uses and areas and encouraging the integration of those uses with other land uses to support the achievement of complete communities.*

**Planning Comment:** The proposed mixed-use residential commercial building will be within a compact built-form and will aid in accommodating the forecasted employment growth of 265,000 jobs for 2041, by providing a maximum 1,303 square metres of commercial space. This will generate additional employment opportunities and diversify the economic base.

**Section 2.2.6** of the Growth Plan provides policies regarding housing.

**Policy 2.2.6.1** *Upper- and single-tier municipalities, in consultation with lower-tier municipalities, the Province, and other appropriate stakeholders, will:*

- a. *support housing choice through the achievement of the minimum intensification and density targets in this Plan, as well as the other policies of this Plan by:*
  - i. *identifying a diverse range and mix of housing options and densities, including second units and affordable housing to meet projected needs of current and future residents; and*
  - ii. *establishing targets for affordable ownership housing and rental housing;*

**Policy 2.2.6.2** *Notwithstanding policy 1.4.1 of the PPS, 2014, in implementing policy 2.2.6.1, municipalities will support the achievement of complete communities by:*

- a. *planning to accommodate forecasted growth to the horizon of this Plan;*
- b. *planning to achieve the minimum intensification and density targets in this Plan;*
- c. *considering the range and mix of housing options and densities of the existing housing stock; and*
- d. *planning to diversify their overall housing stock across the municipality*

**Planning Comment:** The proposed development will support the achievement of complete community, by providing an additional 224 residential units that will contribute to achieving minimum density targets and accommodating forecasted population growth. With the inclusion of both single detached dwelling, townhouse and mixed-use residential units, it will support a diverse range and mix of housing choices and densities in the Smithville Settlement Area.

**Section 2.2.7** *of the Growth Plan provides policies regarding designated greenfield areas.*

**Policy 2.2.7.1** *New development taking place in designated greenfield areas will be planned, designated, zoned and designed in a manner that:*

- a. *supports the achievement of complete communities;*

**Policy 2.2.7.2** *The minimum density target applicable to the designated greenfield area of each upper- and single-tier municipality is as follows:*

- a. *The Cities of Barrie, Brantford, Guelph, Hamilton, Orillia and Peterborough and the Regions of Durham, Halton, Niagara, Peel, Waterloo and York will plan to achieve within the horizon of this Plan a minimum density target that is not less than 50 residents and jobs combined per hectare; and*

**Planning Comment:** Previously stated, the subject lands are identified as a 'Designated Greenfield Area' ("DGA") in the Regional and Municipal Official Plans. The Northwest Quadrant Secondary Plan sets out the vision for DGAs, further described in Section 8.6 of this report. As the size of the proposed stormwater management pond had been increased to accommodate the proposed development, the stand-alone high-density residential planned on the southern portion of the subject lands were removed. An increase in medium density units and the apartment dwelling units within the mixed-use commercial building will compensate the absence of stand-alone high density residential. These built forms will be further compatible with the surrounding low-density uses and small town character of Smithville. Though the proposed development makes some slight adjustments to the location and size of proposed designation, it meets the intent of the vision that the Town planned for DGAs.

The proposed development will support the achievement of a complete community, by integrating a mix of residential and commercial uses and diversifying the built form of housing stock on the subject lands. The proposed dwelling unit density of 21 units per hectare equates to approximately 57 persons per hectare, using the 2016 Census 'average persons per household' ratio for the Population Centre of Smithville of 2.75 persons per household. Therefore, the proposed development will contribute to achieving the minimum combined gross density target of 50 persons and jobs per hectare for Greenfield Areas within the Urban Area of Smithville, by providing a maximum of 224 units on the subject lands,

### 8.3.3 Section 3 Infrastructure to Support Growth

**Section 3.2.6** of the Growth Plan provides policies regarding water and wastewater systems.

**Policy 3.2.6.2** Municipal water and wastewater systems and private communal water and wastewater systems will be planned, designed, constructed, or expanded in accordance with the following:

- a. opportunities for optimization and improved efficiency within existing systems will be prioritized and supported by strategies for energy and water conservation and water demand management;
- b. the system will serve growth in a manner that supports achievement of the minimum intensification and density targets in this Plan;
- c. a comprehensive water or wastewater master plan or equivalent, informed by watershed planning or equivalent has been prepared to:
  - i. demonstrate that the effluent discharges and water takings associated with the system will not negatively impact the quality and quantity of water;
  - ii. identify the preferred option for servicing growth and development, subject to the hierarchy of services provided in policies 1.6.6.2, 1.6.6.3, 1.6.6.4 and 1.6.6.5 of the PPS, 2014, which must not exceed the assimilative capacity of the effluent receivers and sustainable water supply for servicing, ecological, and other needs; and
  - ii. identify the full life cycle costs of the system and develop options to pay for these costs over the long-term.

**Planning Comment:** The inclusion of a storm-water management pond is planned in accordance with the Northwest Quadrant Secondary Plan to accommodate future residential growth on DGA lands, which will achieve minimum density targets. Optimizing the availability of infrastructure and upgrading the watermain along South Grimsby Road 5 and the Streamside Pumping Station will allow for the long term support for the proposed development. Please refer to the FSR, prepared by AJ Clarke for more details.

**Section 3.2.7** of the Growth Plan provides policies relating to Stormwater Management.

**Policy 3.2.7.2** Proposals for large-scale development proceeding by way of a secondary plan, plan of subdivision, vacant land plan of condominium or site plan will be supported by a stormwater management plan or equivalent, that:

- a. establishes planning, design, and construction practices to minimize vegetation removal, grading and soil compaction, sediment erosion, and impervious surfaces; and

**Planning Comment:** As previously stated, the proposed storm-water management pond is a part of the Northwest Quadrant Secondary Plan. The FSR, prepared by AJ Clarke has enlarged the proposed pond to accommodate the proposed and future growth. Therefore, the slight alteration in terms of size to the Secondary Plan is warranted.

### 8.3.4 Section 4 Protecting What is Valuable

Policies within **Section 4 – Protecting What is Valuable** generally address issues of natural heritage, agricultural lands and energy conservation amongst other things.

**Section 4.2.4** provide policies relating to **Lands Adjacent to Key Hydrologic features, and Key Natural Heritage Features**

**Policy 4.2.4.3** Development or site alteration is not permitted in the vegetation protection zone, with the exception of that described in policy 4.2.3.1 or shoreline development as permitted in accordance with policy 4.2.4.5.

**Planning Comment:** Though **Figure 8-1** shows the presence of natural heritage features on the subject lands, the review of aerial photos and a site visit by Colville Consulting has confirmed that there is not much vegetation remaining on the subject lands and therefore it is not a significant woodland. This will be further documented in the Environmental Constraints Analysis Report that will be prepared by Colville Consulting Inc. and submitted under separate cover once summer field work is completed.

**Growth Plan 2019 Conclusion:** The proposed development is consistent with the Growth Plan for the Greater Golden Horseshoe 2019, as it:

- Supports the creation of complete communities, by integrating commercial and residential uses within a compact built form and helping achieve the minimum density targets of the Town of West Lincoln
- Efficiently develops vacant DGA lands within the urban boundary of Smithville, through a compact layout with higher densities to accommodate forecasted residential growth
- Adds a range of housing options to diversify housing stock in the north-west portion of Smithville, through the inclusion of both townhouse, mixed-use apartment and single detached dwelling units
- Fosters economic development, through generating employment opportunities
- Achieve the 50 person density target for DGA lands, by planning and developing for approximately 59 person per hectare
- Does not develop upon a provincially significant natural heritage feature
- Effectively utilizes existing municipal infrastructure to service the proposed development

## 8.4 Region of Niagara Official Plan 2014

The Region of Niagara Official Plan (ROP) sets out policies for the physical, economic and social development within the Niagara Region, which includes the Town of West Lincoln. Currently, the Region is undergoing a Municipal Comprehensive Review to ensure that it implements updated provincial legislation and objectives.

For the purposes of this report, the 2014 consolidated version of the plan available online at <https://www.niagararegion.ca/living/icp/policy-plan.aspx> was used.

The following designations were obtained from the Regional Official Plan schedules and apply to the subject lands.

**Table 8-1:** Region of Niagara Official Plan Designations Applicable to the Subject Lands

SCHEDULE	DESIGNATION
A – Regional Structure	Urban Area Boundary Designated Greenfield Area
D1 – Potential Resource Areas (Stone)	Silurian Formation

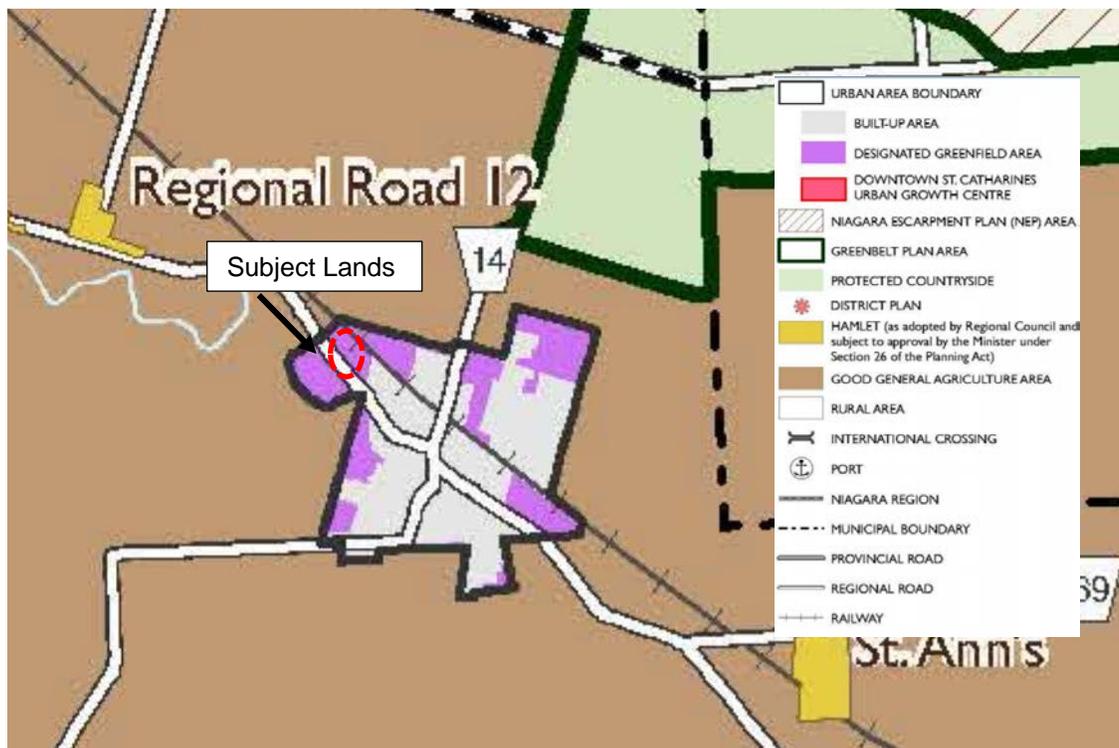


Figure 8-2: Schedule A – Regional Structure Excerpt of the Region of Niagara Official Plan

### 8.4.1 Chapter 2 – Growing the Economy

Chapter 2 of the ROP provides policies for growing the economy. Within Chapter 2, the following strategies objectives are listed.

- Objective 2.1** *To recognize the diversified opportunities and needs in Niagara by balancing both urban development and the conservation of natural resources.*
- a) *A choice of housing and employment locations.*
  - c) *Conservation of natural resources (e.g., fishery habitat, Areas of Natural and Scientific Interest, natural areas, wildlife habitat, waterways, Niagara Escarpment, wetlands, aggregate areas, and woodlots).*

**Planning Comment:** The subject lands contain little vegetation, demonstrated by the aerial photos in Section 3 of this report and a site visit conducted by Colville Consulting. The proposed development includes both residential and commercial land uses and will provide a choice of housing options. The inclusion of commercial space within the mixed-use building at the corner of Regional Road 20 and South Grimsby Road 5 will provide diversified employment opportunities within the neighbourhood.

### 8.4.2 Chapter 3 – Employment

Chapter 3 of the ROP provides policies relating to employment. Subsection 3.D provides policies pertaining to commercial areas.

- Policy 3.D.3** *The Region encourages the local municipalities to plan for commercial uses and commercial space on the basis of a hierarchy of commercial areas*

**Policy 3.D.5** *The Region promotes and will facilitate the revitalization of downtowns and the redevelopment of brownfields, greyfields and commercial areas into mixed-use areas that include a variety of employment, institutional, entertainment, recreational and residential uses.*

**Policy 3.D.8** *The Region recognizes neighbourhood commercial activities as an integral part of Niagara's residential areas.*

**Planning Comment:** The Northwest Quadrant Secondary Plan has designated the south-western portion of the subject lands as 'Commercial'. Including residential uses to the proposed commercial block will create a mixed-use building, supporting existing and future residents in the neighbourhood.

### 8.4.3 Chapter 4 – Managing Growth

**Chapter 4** of the ROP provides policies for Managing Growth. Within **Chapter 4**, the following objectives are listed in **Sub-section 4.A.1**.

**Objective 4.A.1.1** *Direct the majority of growth and development to Niagara's existing Urban Areas.*

**Objective 4.A.1.2** *Direct a significant portion of Niagara's future growth to the Built-up Area through intensification.*

**Objective 4.A.1.6** *Build compact, mixed use, transit supportive, active transportation friendly communities in the Built-up Area and in Designated Greenfield Areas.*

**Objective 4.A.1.10** *Provide a framework for developing complete communities all across Niagara, including a diverse mix of land uses, a range of local employment opportunities and housing types, high quality public open spaces, and easy access to local stores and services via automobile, transit and active transportation.*

**Planning Comment:** The subject lands are located within the existing Urban Settlement Area of Smithville, where growth and development is to be directed. The proposed development would accommodate Niagara's future residential and employment population growth, through developing vacant lands in the DGA in a compact layout, efficiently utilizing municipal water and waste-water systems. The inclusion of commercial space and proposed single detached, apartment and townhouse built forms will add a diversity of land uses and housing types to the north-western portion of Smithville, contributing to achieving complete communities.

**Sub-Section 4.B.2** provides municipal population, household and employment forecasts.

**Policy 4.B.2.3** *The figures in Table 4-1 are forecasts to be used for the purposes of planning and growth management for the individual municipalities.*

Table 4-1: Niagara Region, Population, Household and Employment Forecast by Local Municipality, 2006-2031									
Development Location	Forecast Period	Total Households	Total Population	Total Population With Undercount <sup>1</sup>	Total Population in Households <sup>2</sup>	Persons Per Household (PPH)	Total Employment	Total Employment Including NFPOW <sup>3</sup>	Total Employment Activity Rate <sup>4</sup>
Port Colborne	2006	7,790	18,600	19,300	18,800	2.41	6,150	6,760	0.35
	2011	7,950	18,700	19,400	18,900	2.38	6,440	7,060	0.36
	2016	8,300	19,400	20,200	19,700	2.37	6,580	7,200	0.36
	2021	8,690	20,100	20,900	20,300	2.34	6,720	7,350	0.35
	2026	8,900	20,100	20,900	20,300	2.28	6,930	7,570	0.36
	2031	9,020	20,100	20,888	20,300	2.25	7,140	7,770	0.37
Welland	2006	20,715	50,300	52,300	51,800	2.50	18,550	20,410	0.39
	2011	21,320	51,100	53,100	52,600	2.47	19,300	21,170	0.40
	2016	22,070	52,300	54,400	53,800	2.44	19,770	21,660	0.40
	2021	23,120	54,100	56,300	55,600	2.40	20,320	22,220	0.39
	2026	24,210	55,900	58,100	57,300	2.37	21,110	23,020	0.40
	2031	25,670	59,100	61,464	60,500	2.36	21,710	23,650	0.38
West Lincoln	2006	4,295	13,200	13,700	13,600	3.17	3,490	3,840	0.28
	2011	4,750	14,500	15,100	14,900	3.14	3,780	4,130	0.27
	2016	5,100	15,400	16,000	15,800	3.10	3,990	4,350	0.27
	2021	5,370	16,000	16,600	16,400	3.05	4,410	4,780	0.29
	2026	5,510	16,200	16,800	16,500	2.99	4,710	5,080	0.30
	2031	5,610	16,300	16,990	16,700	2.98	4,930	5,300	0.31

Figure 8-3: Excerpt of Table 4-1 from Region of Niagara Official Plan

**Planning Comment:** The proposed development will aid in accommodating the forecasted population growth of 16,990 and employment growth of 5,330 jobs, by providing approximately 224 residential units to meet the housing needs of a diverse population and a maximum 1,303 square metres of commercial space.

Previous sections provided an analysis of 57 persons per hectare for the proposed development, using the 2016 Census ratio for the Town of Smithville (2.75 persons per household). The figure above shows that the person per household (pph) is currently at approximately 3 pph, therefore the proposed development would result in 63 persons per hectare, higher than originally calculated. As the proposed development would aid in exceeding the minimum density target of 50 persons per hectares for DGAs, the decision to reallocate the planned high-density residential units identified in the Northwest Quadrant into a larger share of medium density dwellings and floors 2 and 3 of the proposed mixed-use commercial residential development is contextually appropriate, as it will be a more compatible use for the surrounding area and provides a maximum of 32 residential units.

**Subsection 4.B.3** provides policies applicable to housing growth, including the following:

**Policy 4.B.3.1** *The Region will maintain the ability to accommodate residential growth within the Regional Market Area:*

- a) *For a minimum of ten years through residential Intensification and redevelopment and, if necessary, through lands which are designated and available for residential development; and*
- b) *For a minimum of three years through lands with servicing capacity that are suitably zoned to facilitate residential intensification and redevelopment or are in draft approved and registered plans.*

**Planning Comment:** The subject lands are within DGA, which are designated and available for residential development. The proposed development of 224 residential units will effectively

accommodate population and household forecasts and residential housing growth in the Region of Niagara, by developing vacant DGA lands.

**Subsection 4.B.4** provides policies applicable to employment growth, including the following:

**Policy 4.B.4.1** *The Region will maintain the ability to accommodate employment growth within Niagara by ensuring that there is a minimum ten year supply of designated employment lands within Urban Areas.*

**Planning Comment:** Though the subject lands are not designated employment lands, the Northwest Quadrant Secondary Plan planned for the south-west portion to be commercial. The proposed inclusion of commercial space will accommodate employment growth forecasts in the Region of Niagara.

### **Designated Greenfield Area**

**Section 4.C** provides policies pertaining to intensification and greenfield growth. **Sub-Section 4.C.5** provides policies relating to Greenfield Development.

**Policy 4. C.5.1** *Designated Greenfield Areas will be planned as compact, complete communities by:*

- a) *Where permitted by scale, accommodating a range of land uses including residential, commercial, institutional, recreational, employment and other uses.*
- b) *Where limited by scale or configuration, making a significant contribution to the growth of the respective Urban Areas as a complete community.*
- c) *Providing opportunities for integrated, mixed land uses.*
- d) *Creating street patterns that are fine grain and in grid pattern, supporting transit and active transportation within the area and to adjacent areas.*
- e) *Ensuring that Greenfield development is sequential, orderly and contiguous with existing built-up areas.*
- f) *Ensuring that the provision of municipal servicing is in accordance with the water and wastewater servicing master plans*

**Planning Comment:** The proposed development will achieve compact, and complete communities, by including a variety of land uses (i.e. commercial, residential and open space) and housing options and providing commercial and residential growth to the Settlement Area of Smithville, through the addition of 224 units and a maximum of 1,303 square metres of commercial space. The proposed development will provide an opportunity to integrate residential, commercial and open space uses to create a mixed-use complimentary neighbourhood.

As seen in the Draft Plan of Subdivision, the proposed street pattern will connect to the future right-of-way access on the adjacent westerly lands. The lands designated as 'Future Development' will be developed after the adjacent parcels have been acquired to ensure that the proposed development will be contiguous with the sequential order of existing development to the east and west of the subject lands.

The proposed development will ensure the efficient provision of municipal services, through a new stormwater management pond and upgrades to the Streamside Pumping Station and watermain along South Grimsby Road 5.

**Sub-Section 4.C.6** provides policy pertaining to the Designated Greenfield density target.

**Policy 4. C.6.1** *The Region will require a minimum combined gross density target of 50 people and jobs per hectare across all Designated Greenfield Areas, excluding the following features within the Environmental Protection Areas and Environmental Conservation Areas in the Region's Core Natural Heritage Systems and any non-developable features designated in local official plans.*

**Planning Comment:** Utilizing the persons per household statistics in **Figure 8-3**, the proposed development would result in 63 persons per hectare and a maximum of 1,303 square metres of commercial space, exceeding the minimum combined gross density target of 50 people and jobs per hectare. Providing more than the minimum density target of 50 persons and jobs per hectare will compensate for proposals that are not able to meet the 50 persons and jobs per hectare target, therefore achieving the minimum combined gross density target across all DGAs.

**Subsection 4.C.7** provides policies relating to achieving the Density Target

**Policy 4.C.7.1** *In order to achieve the planned minimum greenfield density target, official plans shall:*

- a) *Adopt minimum and maximum residential and employment densities in local Official Plans, including distinctions between net and gross density.*
- b) *Include policies for achieving higher residential and employment densities in greenfield areas.*
- c) *Include policies for achieving a mix of housing types and residential densities in greenfield areas.*
- d) *Develop greenfield development guidelines to support local policy direction for greenfield areas.*

**Policy 4.C.7.2** *The Region will monitor the combined 50 people and jobs per hectare target.*

**Planning Comment:** The Northwest Quadrant Secondary Plan envisioned the southern portion of the subject lands as 'Higher Density Residential'. Due to the expansion of the planned stormwater management pond to accommodate proposed and future growth, the proposed development does not include the planned high density residential at that location. The intent and contribution of high-density residential is compensated by additional medium density dwellings and the proposed mixed-use residential commercial building at the corner of Regional Road 20 and South Grimsby Road 5, which provides a maximum of 32 apartment units. The proposed development will still achieve the intent of the high-density residential, as it provides a density of 63 persons per hectare with a more compatible built form and use to support the surrounding area.

There will be a mix of land uses (i.e. residential, open space, commercial) and housing types and densities, ranging from single detached and apartment units to townhouses within the proposed development.

**Sub-Section 4.F.1** provides policies to manage land supply to achieve efficient development.

**Policy 4.F.1.3** *Approval of new Greenfield development will occur where both regional and local existing planned infrastructure can be provided in a financially and environmentally sustainable manner and where such development would not be isolated from existing development.*

**Planning Comment:** Previously stated, the proposed development includes the planned storm-water management pond identified within the Northwest Quadrant Secondary Plan. The pond has been appropriated sized and existing municipal infrastructure will be upgraded to accommodate proposed and future growth.

**Section 4.G** provides the policies applicable to urban growth. It provides the following objectives:

**Objective 1** *Build compact, vibrant, sustainable, integrated and complete communities.*

**Objective 2** *Plan and manage growth to support a strong, competitive and diverse economy.*

**Planning Comment:** The proposed development has been designed to be compact and integrated, through a diversity of land uses and the inclusion of townhouse and apartment dwelling units, thereby achieving a complete community. The addition of commercial to the proposed development will generate employment opportunities and diversify the existing economy. The mix of land uses and reduction in parking will promote sustainable forms of transportation, such as walking and cycling while dwelling forms will increase the vibrancy of the proposed subdivision.

**Objective 3** *Protect, conserve, enhance and wisely use the valuable natural resources of land, air, energy and water for current and future generations.*

**Planning Comment:** The existing vegetation on the subject lands is not considered a provincially significant woodland, as determined by the Environmental Consultants Spring field work site visit.

**Objective 4** *Maximize the use of existing and planned infrastructure to support growth in a compact and efficient manner.*

**Planning Comment:** The proposed development will utilize and upgrade existing municipal infrastructure along South Grimsby Road 5 and the Streamside Pumping Station to support the anticipated growth.

**Objective 5** *Ensure that the Region has an adequate supply of accessible, attractive commercial areas*

**Objective 8** *Recognize the importance of a diversified economic base by establishing flexible policies to support the wide variety of employment types in Niagara.*

**Planning Comment:** The proposed mixed-use residential commercial component will add to the supply of accessible and attractive commercial areas and diversify the economic base within Smithville.

**Subsection 4.G.5** provides policies which outline Niagara Region's Urban Structure.

**Policy 4.G.5.1** *The key components of the Region's Urban Structure are*

*a) Urban Areas composed of:*

*i. Built-up Areas*

*ii. Designated greenfield areas*

**Subsection 4.G.6** provides policies relating to Settlement Areas.

**Policy 4.G.6.2** *Urban Areas will be the focus for accommodating the Region's growth and development. Accordingly, neither the expansion of existing hamlet and village boundaries nor the establishment of new hamlets or villages is permitted.*

**Subsection 4.G.7** provides policies pertaining to Urbans Areas.

**Policy 4.G.7.2** *Urban Areas will be the focus of the Region's long term growth and development.*

**Subsection 4.G.9** provides policies relating to Designated Greenfield Areas.

**Policy 4.G.9.2** *Designated Greenfield Areas are the lands within a settlement area that are not within the Built-Up Area*

**Planning Comment:** As seen in **Figure 8-2**, the subject lands are located within an Urban Area and are designated as a 'Greenfield Area'. As such, the proposed development will focus growth within an Urban Settlement Area.

**Subsection 4.I** provides policies relating to Mixed Use Areas.

**Policy 4.I.1** *The Region encourages the development of mixed use areas in Niagara. Mixed use areas should be planned to accommodate a variety of complementary land uses, including residential, commercial, employment, recreation, institutional, the Core Natural Heritage System, hydrological features and open space*

**Policy 4.I.3** *Mixed use areas should be planned to allow for the mixing of land uses at several scales. Land use could be mixed within neighbourhoods, blocks, parcels and buildings.*

**Policy 4.I.5** *Mixed use areas should be located in both greenfield and intensification areas*

**Planning Comment:** The proposed three-storey mixed-use building will be located on a DGA and includes commercial uses at grade, while the floors two and three are intended for residential apartment units. Overall, the proposed development will accommodate residential, commercial and open space uses.

## **8.4.4 Chapter 7 – Natural Environment**

**Section 7.A.1** provides the following policies for a healthy landscape:

**Policy 7.A.2** *Development should maintain, enhance or restore ecosystem health and integrity. First priority is to be given to avoiding negative environmental impacts. If negative impacts cannot be avoided then mitigation measures shall be required.*

**Policy 7.A.3** *New development, including infrastructure, should be designed to maintain or enhance the natural features and functions of a site*

**Planning Comment:** As confirmed by Colville Consulting during a site visit, there is not much vegetation on the subject lands nor is it provincially significant. Therefore, there are no negative impacts as a result of the proposed development.

## 8.4.5 Chapter 8 – Infrastructure

**Section 8.A** provides the following objectives

**Objective 8.A.1** *To provide a framework and policy linkages for the coordination of infrastructure planning, land use planning, and infrastructure investment to implement this Plan.*

**Objective 8.A.3** *To provide a framework where urban development occurs only in areas with full municipal services and where public/private utility infrastructure has or will be established to serve the anticipated growth and development*

**Planning Comment:** Regional Road 20 and South Grimsby Road 5 have existing water and sewage infrastructure. Upgrades to the existing infrastructure will support the proposed development and safely service future growth and development.

Further, **Section 8.B** provides policies for water and wastewater systems, which includes:

**Policy 8.B.9** *Municipal sewage services and water services are the required form of servicing for development in Urban Areas.*

**Policy 8.B.12** *Construction of new, or expansion of existing, municipal water and wastewater systems should only be considered where the following conditions are met:*

- a) *Strategies for water conservation and other water demand management initiatives are being implemented in the existing service area;*
- b) *Plans for expansion or for new services are to serve growth in a manner that supports achievement of the intensification target and density target in this Plan; and*
- c) *Plans have been considered in the context of applicable inter-provincial, national, bi-national, or state-provincial Great Lakes Basin agreements and are in compliance with the Great Lakes-St. Lawrence River Basin Sustainable Water Resources Agreement.*

**Policy 8.B.22.3** *Prior to approval of development (i.e. plans of subdivision, site plan, etc.) local municipalities shall ensure that required water and wastewater infrastructure and servicing capacity is available to support that development.*

**Planning Comment:** As stated previously, the proposed development will utilize existing municipal water and wastewater systems, with necessary upgrade to support the proposed development. Please refer to the FSR prepared by AJ Clarke for more details.

## 8.4.6 Chapter 9 – Transportation

**Section 9.F** provides policies relating to the Regional Road System.

**Policy 9.F.5** *The Niagara Region will consider the need for noise mitigation measures to address traffic noise from Regional Roads, where required, premised on noise study recommendations per Ministry of the Environment, Conservation, and Parks guidelines for the following situations:*

- a) *New development adjacent to a Regional Road; or,*
- b) *Impact of noise generated by increased traffic on Regional Roads adjacent to established development.*

**Planning Comment:** The subject lands are located along Regional Road 20 to the south. A Noise Study was prepared by dBA Acoustical Consultants and only a few townhouse units required noise fencing to shield amenity space from Regional Road 20. Please refer to the report for recommended mitigation measures.

#### 8.4.7 Chapter 11 – Housing and Community Services

**Section 11.A** provides policies applicable to Residential Areas and Housing.

**Policy 11.A.1** *The Region encourages the provision of a variety of housing types within urban communities and neighbourhoods to serve a variety of people as they age through the life cycle.*

**Planning Comment:** The proposed development will provide a variety of housing typologies, ranging from apartment, single detached, two and three storey townhouses, bungalow townhouses and back-to-back townhouse units.

**Policy 11.A.2** *The Region encourages the development of attractive, well designed residential development that:*

- a) *Provides for active transportation within neighbourhoods with connections to adjacent residential and commercial areas, parks and schools.*

**Planning Comment:** A four-metre wide trail, termed as the 'Rail Trail' will be located in the northern portion of the subject lands. The 'Rail Trail' will extend further than the boundary of the subject lands, connecting to adjacent residential neighbourhoods.

- b) *De-emphasizes garages, especially in the front yard.*

**Planning Comment:** Garages for all of the proposed units will either be in line with the house or more set back than the house façade.

- c) *Emphasises the entrance and points of access to neighbourhoods.*
- d) *Is accessible to all persons*
- g) *Provides an attractive, interconnected and active transportation friendly streetscape.*

**Planning Comment:** A full urban cross-section including curbs will be added to the proposed municipal streets, ensuring accessibility for all persons.

- i) *Balances the need for private and public space*

**Planning Comment:** In addition to private amenity spaces for all proposed dwelling, public open space is located in the north eastern and western portions of the subject lands along the 'Rail Trail' Corridor.

- k) *Encourages a variety of connections based on transportation mode between land uses based on diverse transportation modes, allowing people to move freely between the places where they live, work and play.*

**Planning Comment:** The proposed development will include municipal streets, private streets and pedestrian trails, allowing for a variety of active transportation methods.

**Region of Niagara Official Plan Conclusion:** The proposed development conforms to the Region of Niagara Official Plan, as it;

- Does not develop upon significant natural features
- Supports neighbourhood commercial activities, through the inclusion of a mixed-use residential commercial building
- Helps to achieve a complete community, though a compact layout of diverse land uses and housing densities and typologies
- Assists in accommodating residential and commercial growth, through providing 224 residential units and a maximum of 1,303 retail/office space
- More than exceeds in meeting DGA targets of 50 persons and jobs per hectare, by providing a density of approximately 63 persons per hectare
- Creates a pattern of development that is sequential and orderly to adjacent parcels, by postponing the development of areas designated ‘Future Development’ until adjacent lands have been acquired
- Will optimize the use of existing municipal infrastructure, by upgrading infrastructure along South Grimsby Road 5 and the Streamside Pumping Station

## 8.5 Township of West Lincoln Official Plan

The current Township of West Lincoln Official Plan (the “TWOP”) was approved in 1998 and has been amended through several site-specific amendments and one municipal amendment to implement the Provincial Policy Statement. The Official Plan contains goals, objectives and policies that ensure that future growth preserves and enhances the irreplaceable attributes of the Township. The policies outlined in the Official Plan implement the broader policies of the Regional Official Plan.

For the purposes of this report, the consolidated version of the plan available online at [http://westlincoln.ca/?page\\_id=2287](http://westlincoln.ca/?page_id=2287) was used.

The following designations were obtained from the Township of West Lincoln schedules and apply to the subject lands.

**Table 8-2:** Township of West Lincoln Official Plan Designations Applicable to the Subject Lands

SCHEDULE	DESIGNATION
A – Municipal Structure	Smithville
B-4 – Land Use Smithville	Secondary Plan – Northwest Quadrant Secondary Plan
B-5 – Urban Structure Smithville	Greenfield Area
C-5 – Aggregate and Petroleum Resources	Potential Aggregate Resources
F – Infrastructure & Transportation	Along Arterial Road (Regional Road 20)

### 8.5.1 Section 2 – Preamble

**Subsection 2.4** provides the following goals of the Official Plan:

- (d) *To protect, preserve and enhance the Natural Heritage Features of the Township including water courses, wetlands, other ground and surface water resources, woodlots, aggregate resources, fish and wildlife habitat, areas of natural and scientific interest and important corridors in recognition of significant social, economic and recreational benefits provided.*

**Planning Comment:** Colville Consulting has confirmed through a preliminary site visit that the subject lands contain little vegetation, which is not considered a significant natural heritage feature. The Environmental Constraints Analysis will be provided at a later date, after summer site work has been completed.

- (g) *To accommodate the majority of residential growth of the Township in Smithville in well-planned, residential areas with sufficient housing types and tenures to meet population forecasts; and to promote the efficient use of land and services to meet population growth*

**Planning Comment:** The subject lands are located within the Settlement Area of Smithville. The proposed residential development will have a mix of housing types to accommodate forecasted population, including single detached lots, mixed-use apartment, two-storey and three-storey townhouses, bungalows townhouses and back-to-back townhouse units. The proposed development will efficiently utilize the overall land use area and existing municipal services along South Grimsby Road 5 and the Streamside Pumping Station, by providing a compact and compatible layout of differing housing densities and typologies to meet population growth.

- (j) *To encourage more diversified employment opportunities for residents of the Township through the promotion new and expanding industrial uses, commercial businesses and institutional uses.*

**Planning Comment:** The inclusion of at-grade commercial space will diversify employment opportunities for residents of the Township and Smithville.

- (n) *To ensure the provision of adequate parkland, trails, open space and other indoor and outdoor recreational facilities to meet the need of residents of the Township throughout the plan period of this Official Plan*

**Planning Comment:** Adequate parkland will be provided along the northern portion of the subject lands to meet the needs of projected residents. A 'Rail Trail' is proposed along the CP Rail Corridor, connecting adjacent lands to the west.

- (r) *To ensure that water supply, sewage services, emergency services, and transportation services are planned and developed in accordance with long term planning and growth management objectives.*

**Planning Comment:** Adequate water supply and sewage services will be provided by optimizing and upgrading existing municipal infrastructure at the Streamside Pumping Station and along South Grimsby Road 5. A temporary emergency access with removable bollards to South Grimsby Road 5 will be provided west of the proposed back-to-back units.

## 8.5.2 Section 3 – Basis of the Plan

### Section 3.5 Healthy Communities.

#### Subsection 3.5.1 Objectives

- g) *Provide greenspaces, places to play and gather, and recreational facilities and opportunities for all*
- i) *Create complete neighbourhoods that include mixed densities and affordable housing options for people of all ages and socioeconomic status*

- j) *Provide public spaces, parks and greenspaces where residents can meet, connect and congregate for social interaction.*

### **Policy 3.5.2 Healthy Communities Policies**

- b) *A compact urban form shall be developed that incorporates mixed land uses and promotes active transportation;*

**Planning Comment:** The proposed development will be designed as a complete neighbourhood, providing a mix of low, medium density and apartment mixed-use dwelling forms and incorporating a mix of land uses, including residential, commercial and open space.

## **Section 3.6 Sustainability**

### **Subsection 3.6.3 Objectives**

- b) *To promote the protection and enhancement of the natural environment;*

**Planning Comment:** The aerial images provided in Section 3 of this report and a preliminary site visit by Colville Consulting confirms that the subject lands do not contain a significant amount of vegetation. Therefore, the proposed development does not require any protection or enhancement of any natural environment.

- c) *To increase the efficiency of the use of existing municipal infrastructure;*

**Planning Comment:** The proposed development will utilize and upgrade existing municipal infrastructure located along South Grimsby Road 5 and the Streamside Pumping Station.

- e) *To increase economic development opportunities;*

**Planning comment:** The inclusion of mixed-use residential commercial space will increase employment opportunities and increases economic development in Smithville.

- f) *To increase the amount of parkland within the Township;*

**Planning Comment:** 0.71 hectares of open space are proposed within the subdivision, thereby increasing the amount of parkland within the Township.

- h) *To ensure the development of the Urban Area of Smithville as a complete community;*

**Planning Comment:** The proposed development has been designed in accordance with the Northwest Quadrant Secondary Plan, with some slight alterations to location and size of proposed designations. Though the proposal does not include high density residential in the southern portion of the subject lands, it is compensated by providing a higher number of medium density dwellings and a maximum of 32 residential units within a mixed-use apartment building. This is more compatible with the surrounding small-town character and low density context.

The proposed development will contribute to achieving complete communities, by efficiently utilizing the subject lands to accommodate anticipated population and employment growth in a range of land uses, housing options and densities.

- l) *To promote trails and corridors and linkages across the Township;*

**Planning Comment:** Previously stated, the proposed development will include a trail along the CP Rail Corridor, termed as the 'Rail Trail' which will connect the subject lands to adjacent parcels and the surrounding community, including the Greek Community Centre to the east.

### **8.5.3 Section 5 – Growth Management**

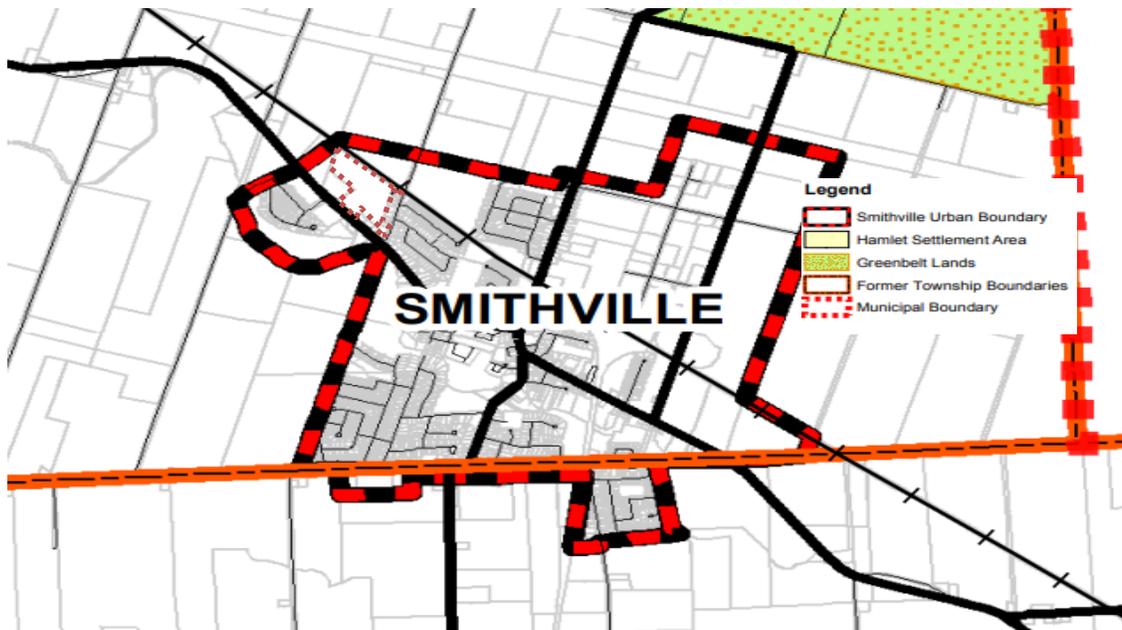
**Section 5.0** provides the objectives relating to the Growth Management Strategy.

### Subsection 5.2 Objectives

- b) *To develop as a complete, balanced community with a diverse mix of land uses, where residents have the opportunity to live, work and play in their community.*
- c) *To ensure an orderly and logical progression of urban development, and an efficient use of land, infrastructure and services.*
- d) *To create a balanced community where employment opportunities are consistent with the available labour force. This is generally considered a ratio of one (1) job for every three (3) residents*
- g) *To direct the majority of the population growth to the Smithville Urban Area and a limited amount to the Hamlet Settlement areas.*

**Planning Comment:** The proposed development will include residential, commercial and open space uses, where existing and future residents can live, work and play within their community. The commercial component with a maximum of 1,303 square metres of retail/office space will be located at an optimal location, so existing residents can access neighbourhood commercial activities. It will provide additional employment opportunities to the Town of West Lincoln.

As seen in **Figure 8-4**, the subject lands are located within the Smithville Urban Area. The proposed development will provide density on vacant lands within the Urban Area Boundary, promote a compact urban form and utilize existing municipal infrastructure, thus being an efficient use of land, infrastructure and services. The proposed layout and inclusion of 'Future Development Areas' will ensure an orderly and logical progression of development, whilst being compatible with surrounding existing built form and adjacent parcels.



**Figure 8-4:** Excerpt of Schedule A - Municipal Structure of the Township of West Lincoln Official Plan

- h) *To ensure that adequate municipal sewer and water services are available in Smithville to accommodate the Township's projected growth.*

**Planning Comment:** Adequate municipal sewer and water services will be made available for the proposed development and projected growth in the 'Designated Greenfield Areas' ("DGA"), through upgrading existing infrastructure along South Grimsby Road 5 and the Streamside Pumping Station.

### Subsection 5.3 Population and Employment Forecast Policies

- a) *The projected population for the Township of West Lincoln by the year 2031 is 16,990 people. The Smithville Urban Settlement Area will accommodate the majority of residential and employment growth within the municipality while limited growth may occur within the Hamlet Settlement areas. The Urban Settlement Area of Smithville includes a built up area, which reflects the limit of growth on June 16, 2006 and Greenfield lands which were the undeveloped lands on June 16, 2006 inside the urban boundary. The built boundary is shown on Schedule B*
- c) *The projected employment for the Township by the year 2031 is a total of (1,150 new jobs) 4,930 jobs. In order to achieve the employment objective of one (1) job for every three (3) residents, an employment target of 126 new jobs will need to be achieved every five (5) years. The majority of those jobs are to occur within the Employment Area designation through Greenfield development and the intensification of existing development. The remainder of the employment will be located in the urban areas through commercial, institutional and government related jobs and in the agricultural area through agricultural and rural related jobs*

**Planning Comment:** The Northwest Quadrant Secondary Plan set out the vision for the DGA lands to accommodate projected population growth. The southern portion of the subject lands were planned for High Density Residential in the Northwest Quadrant Secondary Plan. Due to the expansion of the planned storm-water management pond to accommodate future growth, the proposed development does not include high density residential at that location. Even though there is no high density residential on the southern portion, it is compensated by an increase in medium density dwellings and residential units located within the mixed-use commercial building, thereby achieving the intent of the high-density residential.

The proposed 224 residential units and commercial space will assist in accommodating the projected population growth of 16,990 people and employment growth of 4,930 jobs directed towards the Smithville Urban Settlement Area, by developing vacant Greenfield Lands. Currently, it is too early to identify how many jobs will be proposed as a result of the mixed-commercial development.

- d) *The Township shall forecast for an average gross density of fifty (50) jobs and persons per hectare within the designated Greenfield area.*

**Planning Comment:** Noted in previous sections, the proposed development will entail a density of 57 to 63 persons per hectare, depending on the persons per household statistic used. A maximum 1,303 square metres of commercial space will be included within the mixed-use building. Both residential and commercial built forms will help meet the average gross density of 50 persons and jobs per hectare within the DGA.

### Subsection 5.4 Housing Growth

*The projected housing growth by unit type for the Township is shown below. The Official Plan's land use policies are based on serving this forecasted housing mix. The forecasted mix shown below is the basis for planning the Municipality's long term land needs and housing growth and should not be used on a site specific basis. The Figures below shall be used for undertaking long term planning studies and plans, including, but not limited to housing studies, land needs analysis, and infrastructure plans and studies.*

**Table 5.4 Township Housing Forecast, Unit Mix**

Year	Low	Medium	High	Total Housing Starts
2006	N/A	N/A	N/A	N/A
2007-2011	136	1	6	143
2012-2016	178	9	12	199
2017-2021	289	25	27	341
2022-2026	253	36	36	325
2027-2031	214	49	44	307
<b>2006-2031 Growth</b>	<b>1070</b>	<b>120</b>	<b>125</b>	<b>1315</b>
<b>2006-2031 Mix</b>	<b>81.4%</b>	<b>9.1%</b>	<b>9.5%</b>	<b>100%</b>

**Figure 8-5:** Table 5-4 Township Housing Forecast, Unit Mix Excerpt of Town of West Lincoln Official Plan

**Planning Comment:** As seen in **Figure 8-5**, the unit mix in the Town of West Lincoln is primarily low density. The proposed development will be providing a mix of densities. 45 single detached units are proposed, which will aid in accommodating the Low Density forecasted growth for 2031. The proposed two-storey, three-storey, bungalow and back-to-back townhouse units are considered medium-density, therefore providing 147 medium density units. This will accommodate forecasted Medium Density household growth.

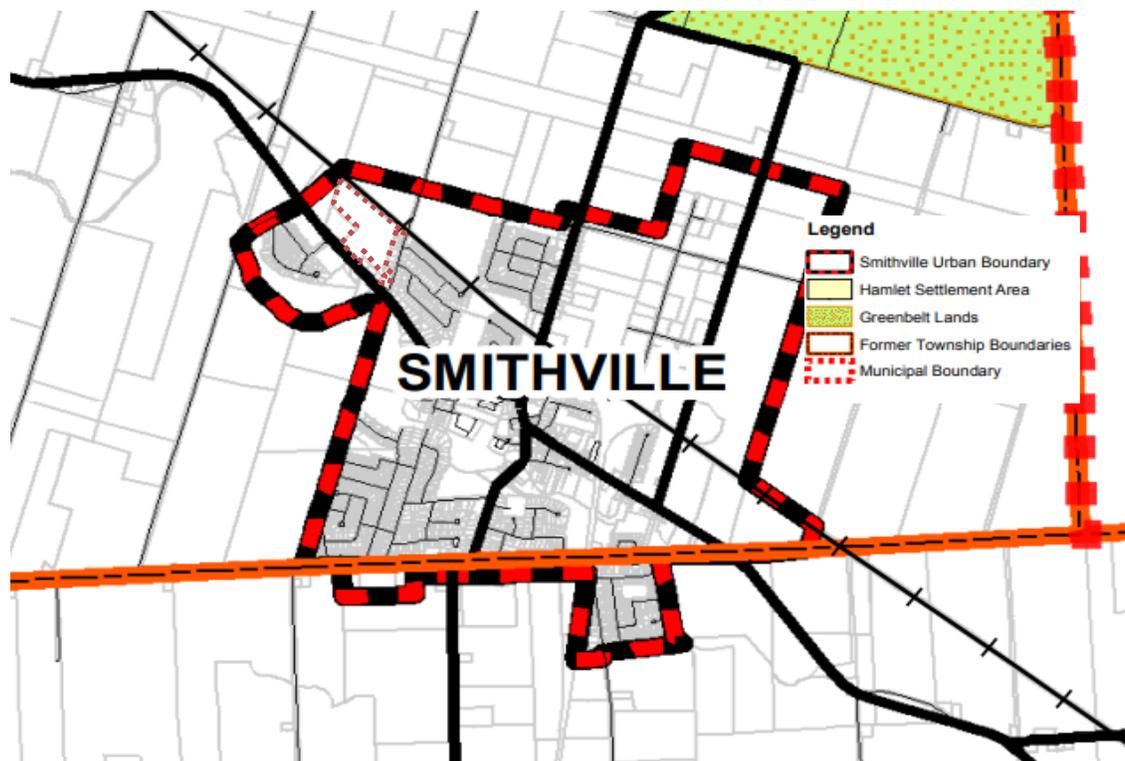
The Northwest Quadrant Secondary Plan planned ‘High Density Residential’ for the southern portion of the subject lands. This will not be able to be accommodated at this location, due to the expansion of the planned storm-water management pond to support anticipated growth. Nonetheless, additional medium density dwellings and inclusion of 32 mixed-use apartment units in the ‘Commercial’ designation will compensate for the high-density residential uses and aid in accommodating projected high density household growth. The proposed mixed use and medium density dwellings are more compatible with the surrounding built form.

Overall, the proposed development will promote a more heterogeneous unit mix for the Township.

### **Subsection 5.6 Urban Settlement Area Structure**

- a) *Smithville is the Urban Settlement Area in the Township of West Lincoln and is the focus for urban development on full municipal services. Any expansion to the Urban Settlement Area boundary is subject to a Municipal Comprehensive Review and the requirements of Section 5.7. The structure of the Urban Settlement Area is comprised of: the built-up area, intensification areas within the built-up area, greenfield areas, the core natural heritage system, and transportation corridors*

**Planning Comment:** As seen in **Figure 8-6**, the subject lands are located within the Smithville Urban Settlement Area. The subject lands were brought within the Urban Settlement Area Boundary, through a Town initiated Official Plan Amendment.



**Figure 8-6:** Excerpt of Schedule A - Municipal Structure of the Township of West Lincoln Official Plan

d) *Greenfield Areas are larger tracts of undeveloped lands that are intended to accommodate new residential and employment growth. Greenfield areas are required to meet an overall density of 50 persons and jobs per hectare, it is realized that not every site will be able to individually achieve that target; however, the Township will closely monitor Greenfield developments to ensure that the Township will meet the overall Greenfield target.*

**Planning Comment:** Depicted in **Figure 8-7**, the subject lands are designated as a Greenfield Area, which are intended to accommodate new residential and employment growth. The Northwest Quadrant Secondary Plans sets out the plan for how DGAs will accommodate forecasted growth. The Secondary Plan envisioned that the southern portion of the subject lands would include a high density residential component. The high density residential was reallocated to a larger share of medium density dwellings and the proposed mixed-use commercial residential building. By providing 224 residential units and a maximum of 1,303 square metres of commercial space, the proposed development will meet the objective of ‘Greenfield Areas’ and will more than exceed the overall required density of 50 persons per hectare, by individually meeting a density of 57-63 persons per hectare. It is unknown at this time how many job opportunities will be produced as a result of the proposed development.

g) *The objectives of the Urban Settlement Area Structure are to direct new growth and redevelopment to the appropriate areas of the Urban Area, and to guide the form and density of the growth to achieve a complete community that is vibrant, well-connected, and prosperous. Guiding growth is more than just densities and land use designations, the aesthetics of the development are important in creating a livable community. As such, the Township will prepare Urban Design Guidelines for the Downtown Core and may prepare more detailed Urban Design Guidelines for Infill Development and other forms of development within Smithville to supplement the policies of this Plan.*

**Planning Comment:** The proposed development mostly adheres to the Northwest Quadrant Secondary Plan, which directs new growth and uses to appropriate areas in the DGA. The planned high-density residential was not appropriate in the southern portion of the subject lands, because it does not reflect the character of the settlement area and will be incompatible with the surrounding built form (low density residential and institutional). Therefore, the high density residential units were re-allocated to more medium density dwellings and a three-storey mixed-use commercial residential building located at the corner of the Regional Road 20 and South Grimsby Road 5. The proposed three-storey mixed-use development is vibrant and a more compatible built form, as it provides the residential units required to accommodate projected growth, while providing neighbourhood commercial uses to support existing and future residents.

### **Subsection 5.10 Greenfield Strategy**

*Greenfield Areas are intended to be developed in such a way as to create more compact, multi-modal, mixed-use communities with a range of housing types and to develop employment lands in an efficient manner. The policies for Greenfield Areas are as follows:*

- a) *The Township will require a minimum combined gross density target of 50 people and jobs per hectare across all Designated Greenfield Areas. Gross density shall mean the jobs and persons per hectare of development excluding the following features and any non-developable features designated in this Official Plan:*
  - i. *Wetlands*
  - ii. *Significant woodlands*
  - iii. *Significant valley lands*
  - iv. *Areas of natural and scientific interest*
  - v. *Habitat of endangered species and threatened species*
  - vi. *Significant wildlife habitat*
  - vii. *Fish Habitat*
  - viii. *Floodplain areas*

**Planning Comment:** The subject lands do not include the features listed above, as confirmed by aerial photos and a preliminary site visit conducted by Colville Consulting. As stated previously, the proposed development of a maximum of 224 residential units and 1,303 square metres of commercial space will more than exceed the minimum combined gross density target of 50 people and jobs per hectare for all Designated Greenfield Areas 'DGA'. The calculated density for the subject lands is from 57 to 63 persons per hectare.

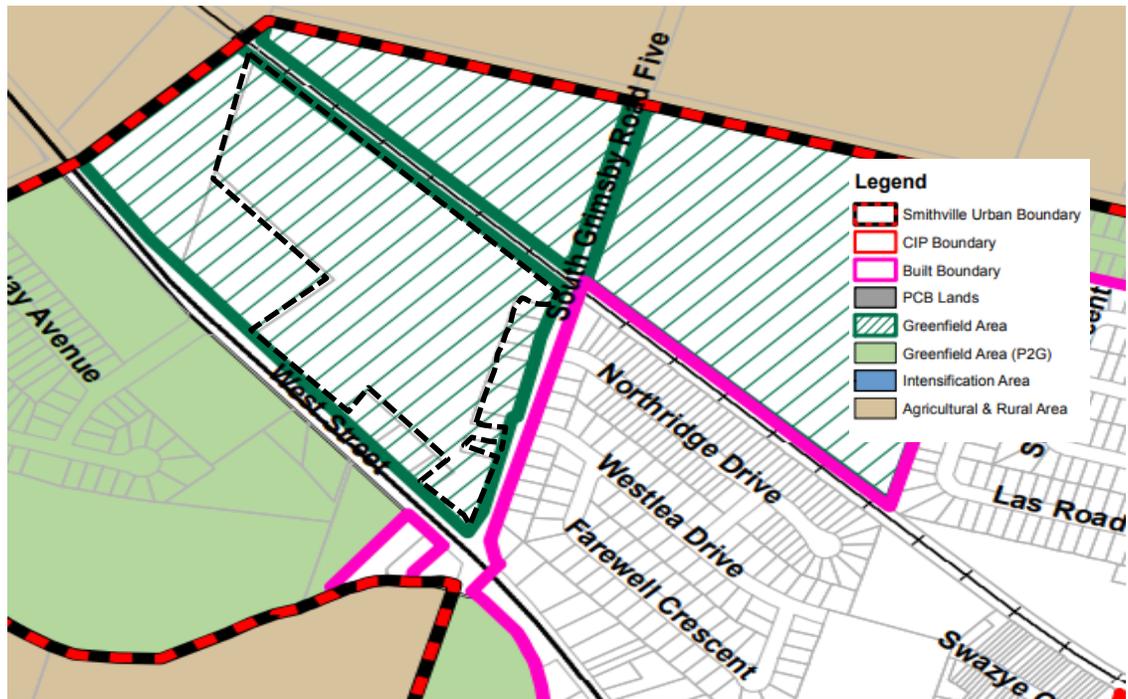


Figure 8-7: Excerpt of Schedule B-5: Urban Structure Smithville of the Town of West Lincoln Official Plan

#### 8.5.4 Section 6 – Urban Settlement Areas

Settlement areas in the Township of West Lincoln include an Urban Settlement Area (Smithville) and Hamlet Settlement Areas. Section 6.1 includes general residential policies for the Urban Settlement Area. The remainder of Section 6 includes policies for development in Smithville. Section 7 includes all policies for the development of Hamlet Settlement Areas. The establishment of new settlement areas is not permitted.

##### Subsection 6.1 General Residential

##### Subsection 6.1.2 Objectives

- b) To promote higher density residential development, and mixed use development/redevelopment in appropriate locations in Smithville;
- c) To encourage and facilitate the production of a range of dwelling types and ownership forms, including housing that is more affordable to the existing and future residents of West Lincoln; and
- d) To encourage high quality design that is environmentally sustainable and is compatible with the character and image of the adjacent buildings.

**Planning Comment:** Higher density development was proposed on the southern portion of the subject lands within the Northwest Quadrant Secondary Plan. The planned high-density residential was not appropriate in the southern portion of the subject lands, because it would be incompatible with the surrounding built form of low density residential and institutional as well as the character of Smithville. In addition, it would not possible given the increased size requirements of the proposed storm-water management pond. The loss of the higher density residential will be compensated through a larger share of compact medium density dwellings and the inclusion of 32 mixed-use apartment units at an optimal location, situated at the corner of Regional Road 20

and South Grimsby Road 5. The mixed-use development is more contextually appropriate and will provide complementary neighbourhood services to existing neighbourhoods. Furthermore, 147 medium density units will also provide the residential units required to accommodate project growth and accommodate the loss of higher-density residential development.

The proposed development will facilitate the production of a variety of dwelling types, ranging from single detached, apartment, two and three townhouses, back-to-back and bungalow townhouse units on vacant lands, designated as a DGA. The range of dwelling types will add to the quantity and diversity of dwelling units within Smithville.

### **Subsection 6.1.3 General Residential Policies**

*The following policies may be applicable to the residential designation of the Urban Settlement Area. Every development shall have access to an open, improved public road which is maintained on a year round basis*

**Planning Comment:** The Draft Plan of Subdivision lays out the land and area for 2 proposed municipal right-of-way to provide vehicular and pedestrian access to residential units. The proposed municipal roads will include curbs and sidewalks. The townhouse blocks will connect to the proposed municipal roads, through private roads which will then feed into Regional Road 20.

### **Subsection 6.1.4 Adequacy of Municipal Services**

*In all instances of development or redevelopment, the adequacy of municipal services to serve the proposals will be evaluated. All development in the Urban Area of Smithville shall be provided with municipal water and sewer service. Private services are not permitted in Smithville. Exceptions may be permitted for commercial or industrial developments which are appropriate for private services as an interim approach while full services are being planned and constructed.*

**Planning Comment:** Adequate municipal services will be providing, by utilizing and upgrading the existing municipal water and sewer services along South Grimsby Road 5 and at the Streamside Pumping Station.

### **Subsection 6.2 Residential (Smithville)**

*The Urban Settlement Area of Smithville is composed of residential, commercial, employment, institutional, recreational and other services related uses. Further, the plan delineates the built up area, greenfield areas, intensification areas, natural environment features, public services and transportation facilities*

**Planning Comment:** As seen in **Figure 8-7**, the Urban Structure of Smithville delineates the subject lands as a DGA. **Figure 8-8** identifies that the subject land's land use will be informed by the NorthWest Quadrant Secondary Plan, outlined in the next section.

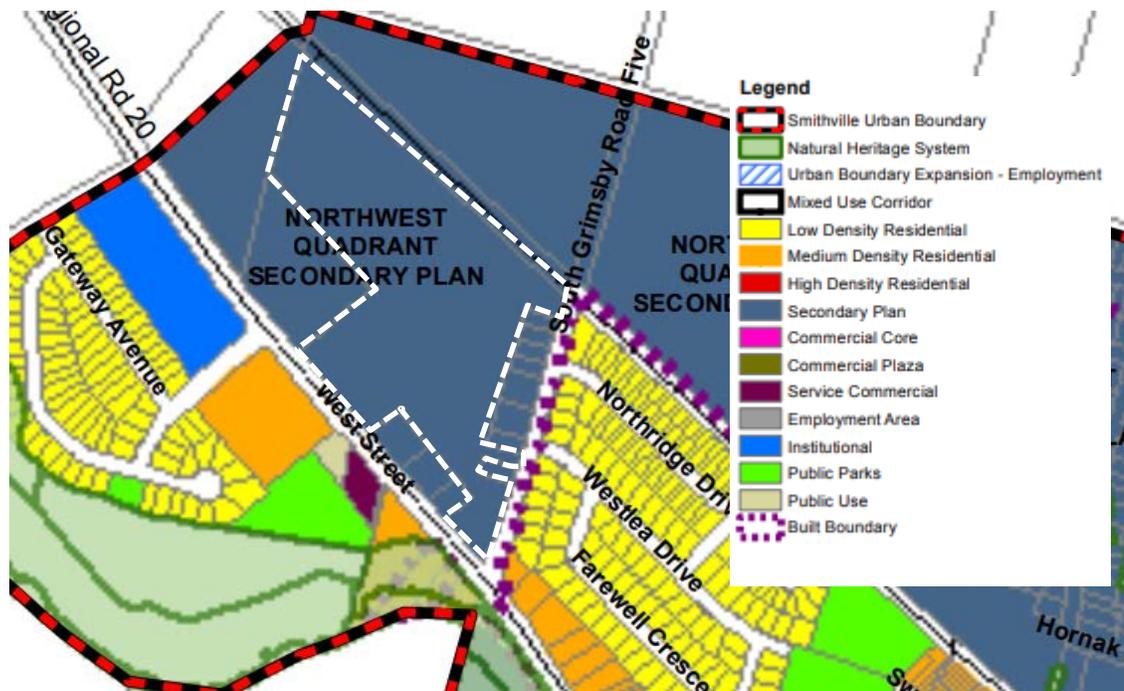


Figure 8-8: Excerpt of Schedule B-4 – Land Use Smithville of the Township of West Lincoln Official Plan

## Subsection 6.6 Design Policies for Residential Neighbourhoods (Smithville)

### Subsection 6.6.1. Objectives

The Township's objectives for appropriate design standards are:

- a) *To enhance the livability and physical appeal of Smithville through the quality, layout and attractiveness of the public streetscape and private spaces and buildings.*

**Planning Comment:** The proposed layout and dwelling type diversity development will enhance the physical appeal of Smithville and create an identifiable image at the westerly entry point of the settlement area boundary. The various housing typologies will create visual interest along the public streetscape. This is further discussed in the Urban Design Brief as prepared by Lindtack Architects.

- c) *To create a built environment within the designated Greenfield areas and all Intensification Areas, which provides:*

- i. *Sensitive integration of new development with existing development;*

**Planning Comment:** The above objective has been carefully considered throughout the design of the proposed development and neighbourhood. Future Development Areas, adjacent to existing development and adjoining parcels have been identified to be deferred until development on adjacent parcels are acquired to ensure that there is a smooth integration between new and existing development.

- ii. *Visual diversity, interest and character through such techniques as façade and roof line composition, varied materials and colour separation;*

**Planning Comment:** Visual diversity and interest within the proposed subdivision is achieved through differing dwelling types, which will break up the façade through different roof-line compositions, materials and colours.

- iii. *A well-defined public realm, including an interconnected open space network;*

**Planning Comment:** The public realm is well-defined within the proposed development, through municipal sidewalks and trail improvements. An open space and trail network will be located on the northern portion of the subject lands, providing residents with parkland and a 'Rail Trail' following the CP Rail Corridor.

- iv. *Good transportation access and pedestrian oriented development patterns; and,*

**Planning Comment:** The proposed development will include one access to Regional Road 20. 2 future right-of-way accesses may be provided to South Grimsby Road 5, subsequent to the confirmation of purchase of parcels to the east of the subject lands.

#### **Subsection 6.6.2 Design Policies and Criteria**

- a) *Each development lot or block will:*

- i. *Have frontage on a public road; and,*  
ii. *Be of sufficient size and appropriate configuration to accommodate development that reflects the planning and urban design policies set out in this Official Plan or approved by the Township through the adoption of Urban Design Standards*

**Planning Comment:** The Draft Plan of Subdivision includes two municipal roads, where residential lots will have frontage onto. The townhouse blocks located in the north-western portion of the subject lands will be able to access the municipal roads, through internal condominium roads.

#### **Subsection 6.6.3 Development Standards**

- a) *New development shall be compatible with adjacent and neighbouring built form by ensuring that the siting and massing of new buildings does not result in undue adverse impacts on adjacent properties particularly in regard to adequate privacy conditions for residential buildings and their outdoor amenity areas.*
- b) *To ensure that compatibility is achieved, the implementing Zoning By-law shall establish appropriate setback and yard requirements which recognize the character of the existing build-form.*
- c) *For reasons of public safety, the main entrances to homes and multi-residential buildings shall be clearly visible.*

**Planning Comment:** The layout of the proposed subdivision will ensure compatibility between each dwelling typology. The proposed two-storey townhouses and bungalow townhouses are located within the same area as the massing and height of both dwellings are similar. The single detached residential dwellings will separate the one and two-storey built form (i.e. bungalow and two-storey townhouses) from the three-storey and back-to-back townhouses to reduce any privacy or compatibility concerns. The proposed three-storey mixed-use residential and commercial building will be located at the corner of a major intersection to ensure high-visibility for customers.

- d) *Access from sidewalks and public open space areas to primary building entrances shall be convenient and direct, with minimum changes in grade.*

**Planning Comment:** Access to the proposed public open space is located in an optimal location at the corner of the CP Rail track and South Grimsby Road 5. This will ensure convenient access from adjacent residential neighbourhoods. A grading plan will be submitted during the detailed design stage to ensure that there are minimum changes in grade for access to building entrances from sidewalks.

- e) *To minimize disruptions to traffic flow and to maximize safety and the attractiveness of Arterial Roads, individual direct vehicular access shall be minimized, and, in some cases prohibited.*

**Planning Comment:** There are no proposed dwellings fronting onto the Regional Road 20, therefore there will be no individual vehicular access points, minimizing disruption to traffic flow.

- f) *To enhance the quality and safety of the public streetscape, the construction of parking lots which occupy significant proportions of the at-grade frontage of public roads, shall not be permitted.*

**Planning Comment:** The parking lot for the proposed mixed-use residential commercial building will be located to the rear to ensure an active frontage for at-grade commercial uses and maintain safety of the public streetscape.

- g) *Location of Buildings will reinforce the road, land and block pattern by using the following measures*
- i. *Along the public street, buildings will be aligned parallel to the road;*
  - ii. *Siting and massing of buildings will provide a consistent relationship, continuity and enclosure to the public roads; and*
  - iii. *Buildings located adjacent to, or at the edge of parks and open spaces and other natural features are encouraged to provide visual opportunities for the features.*

**Planning Comment:** The location of proposed dwellings will be aligned parallel to the street pattern, providing continuity along municipal and private roads.

#### **Subsection 6.6.5 Development Criteria for Single Detached and Semi-Detached Houses within Urban Settlement Area of Smithville**

- a) *Buildings must have front and exterior side facades parallel to the road with front doors, windows and entry features facing the road to create a consistent street wall*
- b) *A consistent setback will be established to the main building face from the edge of the right-of-way. The main building face could be the main front wall, or a significant element such as a roofed porch or veranda but not a garage.*
- c) *Garages shall be encouraged to be set behind or flush with the main building face or porch or accessed from a rear lane. Garages will not dominate the streetscape and shall occupy less than half of the width of the lot.*
- d) *Corner lots and homes facing or abutting parks are priority lots within the development. The design of these homes shall include the following considerations:*
- i. *Where sides or flankage of buildings are visible, they will have windows, materials, and other architectural treatments equal to the front elevation of the house; and*
  - ii. *The primary entrance shall be located on the main public street side of a corner lot, and corner windows and wrap-around porches shall be included to emphasize the corner location.*
- e) *Porches, stairs, canopies and other entrance features can encroach into the required setbacks, in accordance with the Zoning By-law.*
- f) *Entry features and other architectural elements shall be incorporated into the front elevation of the house to reduce the visual dominance of the garage and the front drive.*

- g) *Shared or grouped driveways will be encouraged to reduce the amount of asphalt on front yards.*

**Planning Comment:** It is anticipated that the proposed garages for single detached dwellings will be in line with main building façade. The design of the proposed single detached dwellings will be finalized at the detailed design stage. Please refer to the Urban Design Brief for more detailed analysis of the built form and design characteristics.

#### **Subsection 6.6.6 Development Criteria for Townhouses**

- a) *The elevation and design of the townhouses shall provide variation between units, and reinforces common characteristics that visually unite the block.*
- b) *Variety in the design of roofs is required to break up the massing of townhouse blocks.*
- c) *The massing and built form of townhouse units adjacent to single/semidetached dwellings shall be designed with architectural elements to promote visual integration.*
- d) *Garages shall be accessed from a rear lane. Where they are not, garages are encouraged to be paired to allow for more substantial front yard green space. Garages shall not protrude beyond the main front wall or porch of the dwelling unit.*
- e) *Side and rear elevations visible from public areas and public streets shall have upgraded façade treatments and significant architectural elements.*
- f) *Corner unit designs are encouraged to provide significant corner features such as wrap-around porches, wall articulation, turret or bay windows.*

**Planning Comment:** The north-western portion of the subject lands will include majority of the townhouse dwellings, including two-storey and bungalow townhouses. To provide variation and break up the massing, blocks of each dwelling typology are mixed along the streetscape. There are no private garages proposed for these dwelling types.

For the three-storey townhouse dwellings, a compatible interface will be proposed along the yard adjacent to single detached dwellings. Please refer to the Urban Design Brief for more detailed analysis of the proposed design and how it meets the development criteria.

#### **Subsection 6.6.7 Development Criteria for Apartment Buildings**

- a) *Apartment buildings shall be oriented to front the public road with attractive façade features. A substantial portion of the building shall front the public road at a minimum setback.*
- b) *Entrances shall be located and oriented to public roads.*
- c) *Permanent parking, loading and service areas may be located in side or rear yards and set back from the front façade of the building.*
- d) *Rooftop mechanical equipment shall comply with all applicable Provincial legislation and shall be screened with materials that are complimentary to the building.*
- e) *Collection and storage of recyclable waste shall be provided on-site.*
- f) *Where adjacent lands are designated Low or Medium Density Residential, the height of all new buildings within 25 metres of the property line of these designations shall not be greater than 2 storeys above the existing adjacent buildings, or, if vacant, 2 storeys above the maximum permitted height in the adjacent designation. Alternate building height can be considered based on supporting impact studies and through Rezoning.*

- g) *Applications for planning approval shall include preliminary design plans for the development and for neighbouring properties as viewed from the street, to provide perspective and streetscape design features*

**Planning Comment:** The proposed three-storey mixed-use apartment building will be oriented towards Regional Road 20 at a minimum setback to activate the streetscape to support at-grade commercial uses. Entrances and exits will be provided off of South Grimsby Road 5 in order to maintain a consistent streetscape along Regional Road 20. Parking spaces will be located to the side and rear of the building. Please refer to the Urban Design Brief for more detailed analysis of the proposed design and how it meets the development criteria.

### 8.5.5 Section 14 – Infrastructure and Transportation

#### Subsection 14.6 Rail

- a) *All setbacks from the rail line shall be in accordance with the following:*
- i. *All proposed development within 500 metres of an active railway right-of-way may be required to undertake noise studies to the satisfaction of the Township and the Region in consultation with the appropriate railway, and shall undertake appropriate measures to mitigate any adverse effects from noise that are identified.*
  - ii. *All proposed development within 75 metres of an active railway right-of-way may be required to undertake vibration studies, to the satisfaction of the Township and the Region in consultation with the appropriate railway, and shall undertake appropriate measures to mitigate effects from vibration that were identified.*
  - iii. *All proposed development adjacent to an active railway shall ensure that appropriate safety measures, such as setbacks, berms and security fencing, are provided to the satisfaction of the Township, in consultation with the appropriate railway.*

**Planning Comment:** The proposed development is within 500 metres of an active CP Railway Line. The proposed residential dwelling will be setback from the railway, by open space and trail connections. The recommended measures within the Noise Study, prepared by dBA Acoustical Consultants will be implemented to mitigate any adverse effects from noise.



Figure 8-9: Excerpt of Schedule F: Infrastructure and Transportation of Town of West Lincoln Official Plan

## 8.5.6 Section 15 – Mineral Aggregate and Petroleum Resources

### Subsection 15.1.3

- d) *Schedule 'C-5' illustrates the potential of Mineral Aggregate Areas as identified by the Ministry of Natural Resources. Detailed investigations may be required to determine the area and extent of the actual deposit and development may only occur in accordance with the following policies*

**Planning Comment:** The Soils Report, prepared by Soil-Mat did not find any evidence of mineral deposits.

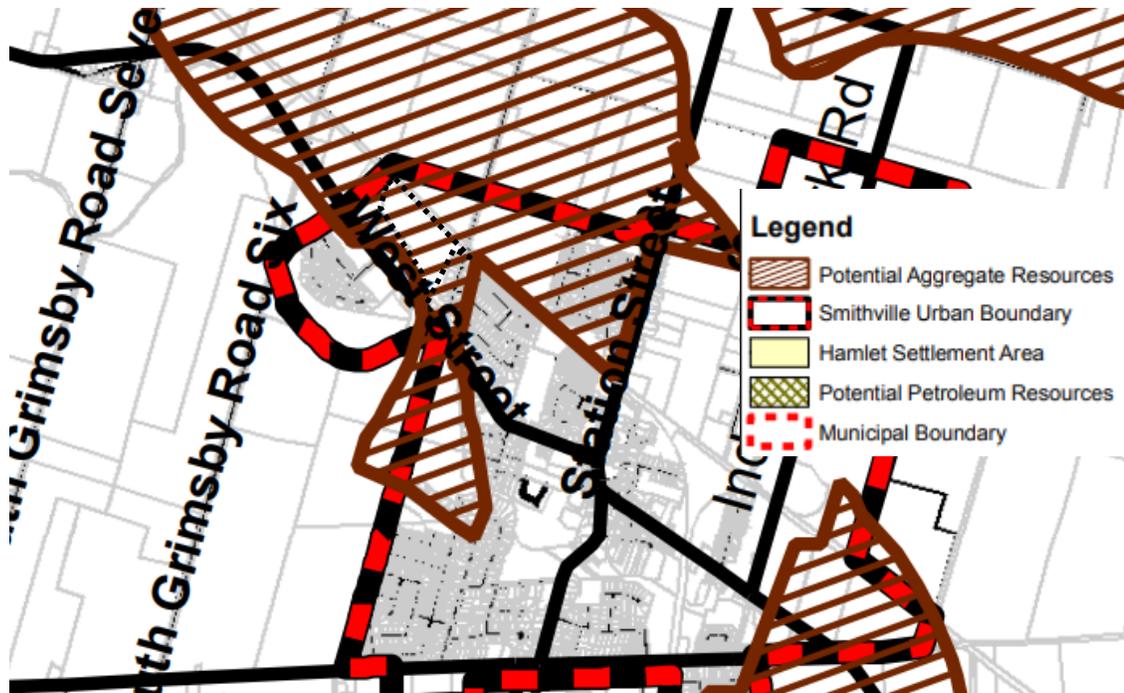


Figure 8-10: Excerpt of Schedule C-5: Aggregate and Petroleum Resources of the Town of West Lincoln Official Plan

**Township of West Lincoln Official Plan Conclusion:** The proposed development conforms to the Township of West Lincoln Official Plan, as it;

- Does not develop upon significant natural heritage features
- Adheres to the municipal Structure, by accommodating residential and employment growth within the Smithville Urban Area
- Develops vacant DGA lands in a compact neighbourhood, featuring a range of uses including residential, commercial and open space
- Consists of a variety of housing types, such as mixed-use residential apartment, single detached, two and three-storey townhouses, bungalow townhouse and back-to-back townhouse units
- More than meets the minimum required overall density for DGA of 50 persons per hectare, as the calculated density is from 57 to 63 persons per hectare
- Creates a well-designed neighbourhood through a layout which emphasizes compatible massing and setbacks, while providing visual diversity along the streetscape

- Mitigates any adverse noise effects, through implementing recommended measures within the Noise Study

## 8.6 Northwest Quadrant Secondary Plan

In 2015, the Township of West Lincoln approved Official Plan Amendment 37 allowing approximately 34 hectares of additional lands to be added into the Urban Boundary of Smithville. The Secondary Plan for the Northwest Quadrant was approved on June 26, 2017 to guide the development of these lands. The policies are contained within Section 6.11.5 of the Township of West Lincoln Official Plan.

### 8.6.1 6.11.5.5 Interpretation

f) *Interpretation*

- In the event of a conflict between the Official Plan and this Secondary Plan, the policies of the Secondary Plan shall prevail*
- Where the general intent of this Secondary Plan is maintained to the satisfaction of the Municipality, adjustments to the size and location of the land use designations and adjustments to the location of new streets will not require an amendment to this Secondary Plan*

**Planning Comment:** On June 12, 2017, the Township of West Lincoln brought a recommendation report to the Planning Building and Environmental Committee to adopt the Final Secondary Plan for the North-West Quadrant. The Town's Secondary Plan for the subject lands is shown in **Figure 8-11**.

During this presentation, concerns regarding the Township's Secondary Plan concept were presented. At the Planning Committee Meeting, it was confirmed by Township staff that there would be flexibility with the Town's version of the Secondary Plan. A copy of the minutes has been attached in **Appendix G**, stating that there is flexibility within the plan. This is confirmed by the policy above, that adjustments can be made to the size of location of land use designations and streets without necessitating an Official Plan Amendment. Flexibility is warranted as the stormwater management pond need to be expanded to accommodate the proposed and future growth of the subject lands and adjacent properties. As well, due to the awkward shape of the parcel the setbacks required from the railway, incorporating Town road standards and creating buildable lots, flexibility to this plan is needed and warranted.

**Figure 8-12** shows that the proposed Land Use Plan of the proposed development. Street H is designed as the concept presented within the Secondary Plan. Street G also follows the street pattern, however it will terminate on either end as temporary cul-de-sac until the adjacent lands have been conceptualized or purchased. The location of Street F has been moved to west and is shorter than the street pattern in the Secondary Plan, however the intent to provide another access to South Grimsby Road 5, remains the same. Within the current Land Use Plan, Street F will terminate as a temporary cul-de-sac until conflicts with the adjoining parcels have been resolved. Therefore, there is only a slight adjustment to the location of new streets within the proposed development.

The proposed low density residential area remains the same as the Secondary Plan, however adjustments have been made to the size and location of the land use designation. **Figure 8-12** shows that a portion of low density area will be redistributed to along Street G. The redistribution of the low density residential area to Street G will provide an adequate transition to the existing Greek Community Centre. Also noted in the Land Use Policy section of the Secondary Plan below, townhouses are considered as part of the low density land use area, therefore the proposed low density residential designation maintains the intent of the Secondary Plan and only minor adjustments have been made to the placement of the designation.

In the Formal Pre-Consultation and Preliminary Review of the Concept Plan, municipal staff have indicated that the layout of the units within the proposed medium density zone, including all townhouse typologies is acceptable, even though the designation covers more area than what is shown in the secondary plan. The increase in the medium density area is warranted by the decision to not develop a high density residential use at the intersection of Regional Road 20 and Street H due to in part, incompatibility concerns from surrounding low density neighbourhoods. The proposed high-density residential built form does not reflect the small town character of Smithville.

The intent and contribution of the high density residential is also compensated by the proposed three-storey mixed-use residential commercial building at the corner of Regional Road 20 and South Grimsby Road 5. The vision of the commercial area remains, with the addition of residential units on floors 2 and 3 of the building. This is a more compatible built form, than high density residential.

Comparing the figures below, the size of the proposed stormwater management pond had to be increased to accommodate anticipated growth, which is another reason behind the removal of the high-density residential. Please refer to the FSR for more details.

Overall the proposed development maintains the intent of the land-use designations with some slight adjustments to the size and location of some uses.

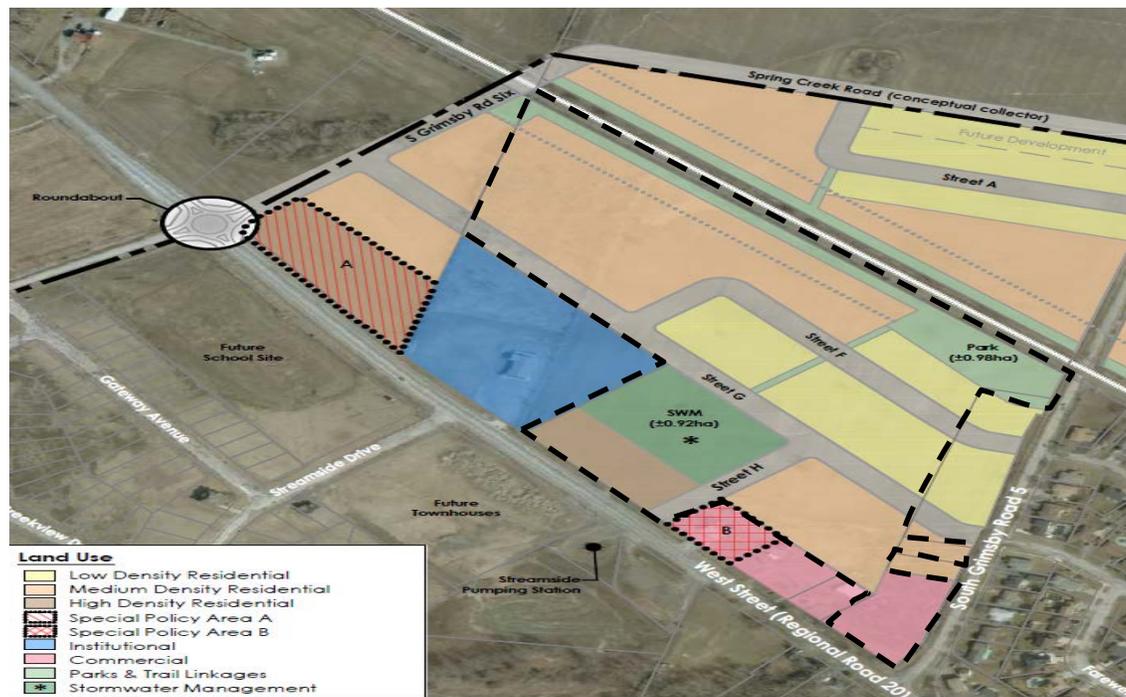
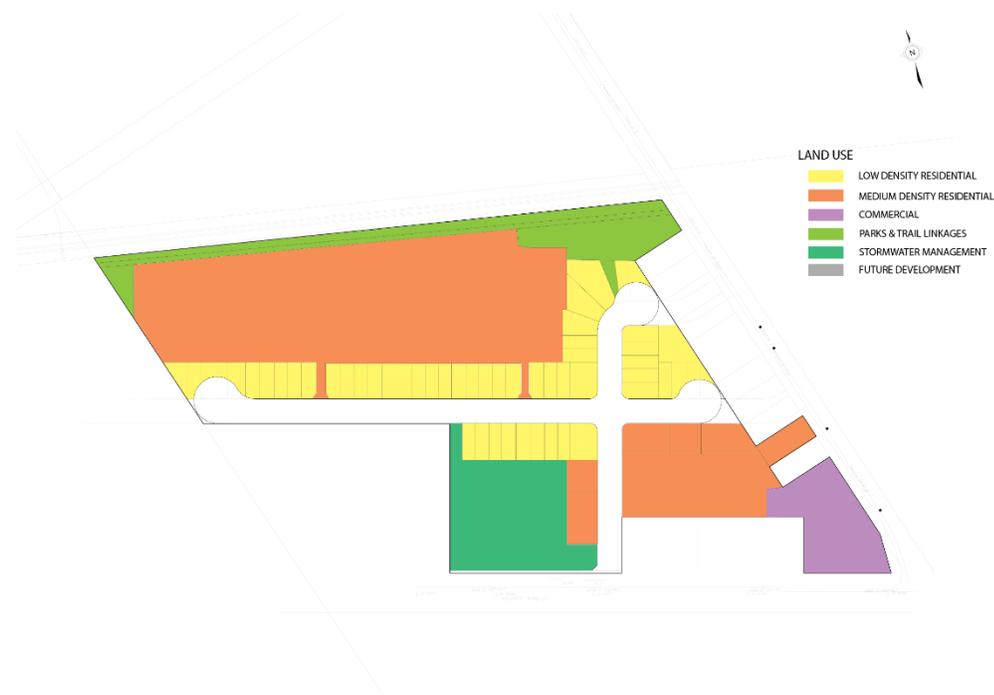


Figure 8-11: North-west Secondary Plan Land Use Plan



**Figure 8-12:** Proposed Land Use Plan, prepared by IBI Group

### 8.6.2 6.11.5.1 Background

#### *Vision*

*The Northwest Quadrant area totals 34 hectares in size and represents the only remaining Greenfield Area in the Township of West Lincoln intended to accommodate non-employment uses. The purpose of the Northwest Quadrant Secondary Plan is to guide the detailed planning and development in a manner that integrates new development with the existing urban neighbourhoods and adjacent rural lands while respecting and protecting the surrounding environmental features and agricultural uses.*

*The Northwest Quadrant is proposed to be developed primarily as a complete community with residential uses; an integrated parks and open space system; and, local serving commercial uses. The Northwest Quadrant is expected to function as a prominent gateway into the urban area of Smithville and has the potential to accommodate a minimum of 570 new residential units by 2031.*

*Accordingly, it is the vision of the secondary plan that the development of the Northwest Quadrant shall reflect the small town character of Smithville and provide an attractive, high-quality, safe, sustainable, interconnected, and pedestrian-friendly community for future residents of all ages and abilities to enjoy. Ultimately, the Northwest Quadrant will become an urban neighbourhood with a strong pedestrian focus enhanced by public green space and trail linkages and a small scale commercial function to support the needs of the neighbourhood residents.*

**Planning Comment:** The proposed development will follow the vision of the Northwest Quadrant Area, as the layout of the neighbourhood will integrate with existing adjacent development to ensure that there are no compatibility issues. To ensure compatibility, the planned high-density residential was reallocated to medium density dwellings and a mixed-use residential building to better reflect the small town character of Smithville. It will contribute to meeting the minimum 570 new residential unit growth requirement, by providing approximately 224 residential units. The proposed development has been designated to function as a complete community in itself, by

including parkland and open space in the northern portion of the subject lands, which will link to the Rail Trail.

a) *Community Structure*

- i. *The Community Structure Plan is illustrated in Schedule A and sets out the overall urban structure for the Northwest Quadrant Secondary Plan area. The Community Structure is based on the design principles identified in Section 6.11.5.1 c) and provides a neighbourhood structure organized around an identified future gateway node; a local road pattern which connects to the existing collector road network in the surrounding area; and, an integrated open space network.*
- ii. *The lands are identified as Designated Greenfield Area in the Regional Official Plan and are currently designated Greenfield Area in the Township of West Lincoln Official Plan. The Northwest Quadrant lands will be developed as a primarily low and medium density residential neighbourhood, with the opportunity to develop a limited amount of high density residential and local serving commercial uses at key locations. It intended that the identified commercial block be developed to serve the day to day needs of future residents of the Northwest Quadrant Neighbourhood and residents of the adjacent subdivision to the east. The gross density target for new residential developments in the designated Greenfield area is a maximum of 20 to 30 units per hectare. The Region of Niagara Regional Official Plan requires that residential lands in designated Greenfield areas will be planned and designed to achieve the minimum density target as set out by the Region and the Province through the Places to Grow Growth Plan. It is recognized that density targets are to be achieved across the designated greenfield area and the Region.*
- iii. *To achieve density targets and create complete communities that provide for housing needs and choice within the Township, the Township's Official Plan emphasizes the development of Greenfield areas to create more compact, multi-modal, mixed-use communities with a range of housing types to develop an orderly and logical progression and which utilizes land, infrastructure and services efficiently. The community structure and associated land use designations allow for development yield of between 550 and 570 units (population of approximately 1,700- 1,750 people), which would ensure that the required minimum greenfield density target of 50 people and jobs per hectare will be achieved. Development on these lands could achieve higher densities, should individual development proposals propose development that reaches the maximum level permitted by these land use designations.*

**Planning Comment:** The proposed development will consist of low and medium density dwelling types with a maximum 32 mixed-use apartment, 45 single detached and 147 townhouse dwellings, including two storey, three-storey, bungalow and back-to-back units. The high-density residential portion was removed as it did not fit with the character of neighbouring areas and Smithville and was redistributed to a larger share of medium density dwellings and mixed-use apartment units.

A limited amount of commercial space will be provided within the three-storey mixed-use building at the corner of Regional Road 20 and South Grimsby Road 5. This is an optimal location to serve the needs of future residents of the proposed subdivision and existing residents in surrounding neighbourhoods.

The layout of the neighbourhood will be compact to ensure a logical progression of densities to reduce any compatibility issues. The proposed dwelling unit density will be 21 units per hectare, between the maximum of 20 to 30 units per hectare. This equates to approximately 57 – 63 persons per hectare, achieving the minimum density target for DGA set out in the Growth Plan, Regional and Municipal Official Plans.

b) *Land Use Designations*

*Lands within the Secondary Plan Area are designated one, or more, of the following land use categories as depicted on Schedule “E-4”:*

- i) Low Density Residential,*
- ii) Medium Density Residential,*
- iii) Medium Density Residential/ High Density Residential,*
- iv) Institutional,*
- v) Commercial,*
- vi) Future Development,*
- vii) Parks; and,*
- viii) Stormwater Management*

*Schedule “E-4” also identifies several special policy areas, where certain exceptions and permissions exist, which consider existing zoning permissions and the potential for future urban area expansion to the west. The land use designations for the Secondary Plan are intended to complement the broader land use designations provided in the Township of West Lincoln Official Plan. In most cases, the land use policies and permissions described in the Secondary Plan are more detailed than those provided for within the Official Plan. Where there are inconsistencies between a particular policy in the Official Plan and the Secondary Plan, the policies of the Secondary Plan shall prevail.*

**Planning Comment: Sections 8.5 and 8.6.1** of this Report has confirmed that the proposed development aligns with the Secondary Plan Land Use Schedule and does not require an Official Plan Amendment.

*c) Community Design*

*The Township of West Lincoln Official Plan (2015 Consolidation) identifies that form and density of growth within the Township’s urban areas should be guided by appropriate design standards intended to achieve a complete community that is livable, vibrant, well-connected, and prosperous. In order to achieve the desired vision, the Township has prepared and adopted Urban Design Guidelines for Smithville which provide enhanced guidance for the physical design of public and private development in Smithville to create an attractive, high-quality, safe, sustainable, interconnected, pedestrian-friendly and transit-ready community.*

*The Secondary Plan is based on neighbourhood urban design principles, aimed at establishing a complete community. These principles have used the urban design guidelines identified in the Township of West Lincoln: Smithville Urban Design Manual as their basis.*

*The Secondary Plan design principles are founded on six key themes aimed at establishing a ‘complete community’:*

- Safety;*
  - Integration and phasing;*
  - Housing mix and density;*
  - Balance;*
  - Open space network; and,*
  - Identity and urban design.*
- d) Design Principles Development in the Northwest Quadrant shall be consistent with the following principles:*

- *Provide a diverse mix of land uses and unit types to support a vibrant neighbourhood*

**Planning Comment:** The proposed development will provide a mix of land uses (i.e. residential, commercial, open space) and a diverse mix of unit typologies, including mixed-use apartment, single detached, two and three-storey townhouse, bungalow townhouses and back-to-back townhouses to support a vibrant neighbourhood and accommodate the needs of a changing demographic.

- *Provide a neighbourhood design concept that considers safety and mitigates railway-oriented impacts*

**Planning Comment:** The neighbourhood design has carefully considered the CP Rail Corridor to the north, by planning an open space system and trail linkage, to provide adequate buffering from the residential dwellings. Further mitigations measures recommended by dBA Acoustical Consultants will be implemented.

- *Develop a neighbourhood at the pedestrian scale to promote social interaction, active streetscapes and an overall walkable neighbourhood*

**Planning Comment:** The mix of land uses, along with the parkland and trail will promote walking and active transportation, so residents can play, live and work in their community. The proposed mixed-use building will be designed with a minimized front yard setback to activate the streetscape along Regional Road 20 to foster social interaction for the at-grade commercial/ retail uses.

- *Establish an open space network of integrated and connected public spaces including parks, trails, stormwater management facilities and natural features*
- *Develop a well-connected network and hierarchy of streets, paths and trails that enhance connectivity and safely accommodate various modes of active transportation, including walking, cycling, and automobile use*
- *Locate parks and recreation spaces that will serve all age groups and physical abilities throughout the neighbourhood and connect them with an integrated active transportation system*

**Planning Comment:** An open space system and trail linkage will be located in the northern portion of the subject lands to serve existing and future residents of all age groups. The 'Rail Trail' will extend beyond the limits of the subject lands to integrate with surrounding neighbourhoods. The hierarchy of private and municipal streets with sidewalks and a trail system will enhance connectivity within the proposed subdivision and accommodate all modes of transportation.

- *Promote compatibility of scale and form between new and existing adjacent development*

**Planning Comment:** Compatibility with adjacent parcels will be achieved as the owner is actively pursuing the purchase of these lots to provide additional dwellings and municipal road connections to South Grimsby Road 5.

- *Create focal points, corridors and activity nodes within the community to enhance way-finding and establish an identifiable community structure*

**Planning Comment:** The proposed mixed-use residential commercial building at the corner of Regional Road 20 and South Grimsby Road 5 will be considered a focal point to the DGA, as it is located at a major intersection. As well, the new municipal road off of Regional Road 20 will also be a focal point to the new subdivision.

- *Promote sustainable design throughout the built environment to promote efficient use of energy, land, and infrastructure through conservation and energy-saving practices and systems*

**Planning Comment:** The proposed development promotes sustainable design through a compact built-form, minimizing additional land consumption and infrastructure.

e) *Urban Design Policies*

- i. *Any proposed development shall address the Urban Design Policies of this Plan and, where appropriate, Policies within the Township of West Lincoln's Official Plan and the applicable built form guidelines established in the Township's Urban Design Manual.*
- ii. *An urban design brief, or design guidelines, which demonstrate how any proposed future plan of subdivision meets the direction of the Township's Urban Design Manual and the Urban Design principles and policies of this Secondary Plan, will be required in support of any such application.*

**Planning Comment:** An Urban Design Brief was completed by Lindtack Architects to address the Town's Urban Design Policies and meet the direction of the Township's Urban Design Manual and Urban Design principles and policies.

- iii. *Development within the Secondary Plan area, including but not limited to areas of public use, shall be designed in accordance with the Accessibility for Ontarians with Disabilities Act, and other applicable Provincial legislation and the Region's Accessibility Design Standards.*

**Planning Comment:** The proposed development will demonstrate AODA compliance in the detailed design stage.

- iv. *Streetscapes throughout the Northwest Quadrant Community are important components of the public realm. Streetscapes will be designed to enhance community character and sense of place.*

**Planning Comment:** The streetscape along the frontage of the proposed mixed-use residential building will be enhanced and designed to activate the public realm, which will support at-grade commercial uses. As well full municipal streetscape features such as sidewalks and street lighting will be provided.

- v. *Residential streetscapes shall be designed to ensure the provision of sufficient on-street parking through creative design solutions such as varying housing types and lotting patterns.*
- vi. *Development throughout the Northwest Quadrant Community shall be encouraged to create a sense of identity and place through the use of various means, including unique building typologies, architectural design treatments, building materials, decorative lighting, decorative street signs, boulevard treatments, gateway features and landscaping elements. Facades should address the public street as a means of creating community identity and a sense of place.*

**Planning Comment:** Municipal roads will be sized sufficiently to accommodate on-street parking. The residential townhouses that front onto private roads will have sufficient visitor parking areas provided, as the road width will not be large enough to accommodate two-way traffic and on-street parking. The proposed development will create a sense of place, by integrating a variety of housing forms, providing visual diversity along the façade of the streetscape. This is result of different architectural design treatments and building materials for each dwelling typology.

- vii. *Gateway nodes are identified in the Township's Urban Design Manual. These nodes are intended to signal a sense of arrival and help create a cohesive community identity for Smithville through high-quality and attractive built form, open space, and markers. Development within lands identified as a Gateway Node shall adhere to the Guidelines for Private Development within Gateway Nodes contained within Section 7.2 of the Design Manual.*

**Planning Comment:** As per the Community Design Framework mapping that is provided in the Urban Design Report as prepared by Linktack Architects Incorporated shows that the Community Gateway node is located at Regional Road 20 and South Grimsby Road 6, this is not a part of our development proposal and does not apply to the subject lands.

- viii. *Throughout the neighbourhood, buildings shall be designed to form a well-defined and continuous street edge with subtle variations in height and setbacks and high quality architectural features.*
- ix. *Human scale buildings will be required and be achieved through siting and orientation of a building on a lot and the distribution of building heights and massing. Consideration of building materials and architectural articulation must have regard for maintaining a human scaled form of development. Building heights shall transition between the higher intensity areas to the surrounding lower intensity areas.*

**Planning Comment:** As per the Urban Design Report that was prepared by Linktack Architects Incorporated, the various buildings will have variations in height and articulated facades to create varied massing and orientation. Garages will be recessed from the front façade to form a well-defined aesthetically-pleasing street edge.

- x. *Decorative lighting, street signage and entrance features are encouraged*

**Planning Comment:** It is our understanding that these features can be incorporated to the detailed design of the municipal and private streets.

- xi. *Buildings on corner lots should articulate facades on both street frontages. Facades of building corners facing public streets should be architecturally consistent in design and materials.*

**Planning Comment:** It is our understanding that corner lots will have covered porches where the design permits to enhance the entrances that will be oriented towards both streets. Please refer to the Urban Design Brief for detailed description.

- xii. *Buildings at terminating vistas will be encouraged to be designed as landmarks, with architectural innovation and quality urban design that reflects the community character. Incorporation of public art and enhanced landscaping will be encouraged at these areas.*

**Planning Comment:** There are no buildings that will be located at terminating vistas. While at the new intersection of Street A and Regional Road 20, there will be the Stormwater Management Pond, it is possible to provide enhanced landscaping to create a gateway into the subdivision.

- xiii. *Dwellings located on lots in visually prominent locations are priority lot dwellings. Priority lots include those lots within Gateway Nodes, on corner lots, at 'T'- intersections, or directly adjacent to or fronting open spaces, parks, trails natural areas or agricultural lands. Priority lot dwellings shall be identified as such on plans of subdivision and the treatment of priority lots shall be described in the design brief to be submitted in support of a development application.*

**Planning Comment:** It is our understanding that these features can be incorporated to the detailed design of the municipal and private streets.

- xiv. *Stormwater management systems shall be integrated into the design of buildings and the landscape where appropriate. The use of Low Impact Development stormwater techniques shall be strongly encouraged.*

**Planning Comment:** The stormwater management pond will appropriately landscaped to provide an aesthetically pleasing streetscape for the adjacent residential uses and along Regional Road 20.

### 8.6.3 6.11.5.2 Land Use Policies

#### a) *General Policies*

- i. *The Northwest Quadrant is planned to achieve an overall minimum density of at least 50 people and jobs per hectare. The Township promotes, and will support, development proposals which would result in increased density beyond the current minimum density target.*

**Planning Comment:** The proposed development will include a maximum of 224 residential units and 1,303 square metres of commercial space. Currently, it is unknown how many jobs will be produced. The density of the development is calculated to be approximately 57 to 63 persons per hectare, which is beyond the current minimum density target of 50 people and jobs per hectare.

- ii. *Residential lands will be designated primarily low density and medium density residential, with some higher density residential, mixed use and local serving commercial uses in appropriate locations*

**Planning Comment:** The proposed development is primarily medium density (i.e. two and three-storey townhouse, bungalow townhouse, and back to back townhouse) and low density (i.e. single detached) in nature. Higher density residential will not be included as part of the proposed subdivision, due to the enlargement of the proposed storm-water management pond. In addition, higher density dwellings are not contextually appropriate and pose incompatibility concerns with surrounding built form and the character of Smithville. To compensate, a maximum of 32 apartment units will be located within a three-storey mixed-use development. Commercial uses will be included at-grade. The location at a major intersection will allow the mixed-use development to service existing neighbourhoods.

- iii. *Development located adjacent to a rail line shall comply with CP Rail Principle Main Line requirements. Development in proximity to air quality/noise and vibration generating uses shall submit appropriate studies pursuant to the guidelines and regulations of the Environmental Protection Act and any applicable regional and Township policies regarding noise and vibration.*
- iv. *In order to address challenges related to development within proximity to a rail line, future development applications are encouraged to refer recommendations contained within the Guidelines for New Development in Proximity to Railway Operations document and apply these principles, where applicable and appropriate*

**Planning Comment:** Please refer to the Noise Study prepared by dBa Acoustical Consultants. The recommended mitigation measures will be implemented to reduce the impact of noise and vibration.

- v. *To be consistent with the Township of West Lincoln Official Plan and to achieve the density target, the Northwest Quadrant Neighbourhood will be planned, designed and zoned to achieve a variety of housing types, styles and lot sizes*

**Planning Comment:** The proposed Zoning By-law Amendment is consistent with the policy above, as the subject lands will be zoned to achieve a variety of housing type and lot sizes in the proposed development.

#### b) *Low Density Residential*

- i. *The planned function of the Low Density Residential designation is to provide opportunity for the development of low-rise residential land uses at lower densities. While the predominant land use with the Low Density Residential designation will be residential, it is intended that complementary non-residential land uses may be permitted to locate within this designation provided they do not impact the ability of the lands to achieve the vision and policies of this Plan and the required overall density target.*

- ii. *Lands designated Low Density Residential may be zoned to permit the following residential uses:*
  - a. *Single detached dwellings;*
  - b. *Semi detached dwellings;*
  - c. *Duplex dwellings;*
  - d. *Linked semi-detached dwellings; and,*
  - e. *Townhomes and other forms of multiple unit ground related housing*
- iii. *In addition to permitted residential uses, lands designated Low Density Residential shall permit the following uses, subject to applicable policies of the Township of West Lincoln Official Plan and the Zoning By-law*
  - a. *Home Occupations;*
  - b. *Public and Private Utilities;*
  - c. *Bed and Breakfast Establishments;*
  - d. *Day Care Facilities;*
  - e. *Places of Worship;*
  - f. *Educational Facilities;*
  - g. *Accessory Apartments; and,*
  - h. *Garden Suites*
- iv. *Low Density Residential uses shall achieve a maximum density of 30 units per hectare.*
- v. *The maximum building height in the Low Density Residential land use designation shall be 2.5 storeys*

**Planning Comment:** Within the Draft Plan, Lots 1-45 contain low density residential uses in the form of single detached dwellings. The maximum density on the subject lands intended for Low Density Residential uses will be 29 units per hectare, therefore staying within the maximum density of 30 units per hectare.

- c) *Medium Density Residential*
  - i. *The planned function of the Medium Density Residential designation is to accommodate a range of medium density housing types including townhouse dwellings, multiple dwellings and communal housing. While the predominant land use within the Medium Density Residential land use designation will be multiple residential, it is intended that complementary non-residential land uses may be permitted to locate within this designation provided they do not impact the ability of the lands to achieve the vision and policies of this Plan and the required overall density target.*
  - ii. *The Township will encourage and support the mixing and integrating of innovative and different forms of housing to achieve and maintain a medium density built form*
  - iii. *Low density residential uses are permitted within the medium density residential area, provided it can be demonstrated that the overall density of the plan area is maintained at a minimum of 50 people and jobs per hectare*
  - iv. *Lands designated Medium Density Residential may be zoned to permit the following residential uses:*
    - a. *Single Detached Dwellings;*

- b. *Semi-Detached Dwellings;*
- c. *Duplex Dwellings;*
- d. *Triplex Dwellings;*
- e. *Townhomes and other forms of multiple unit ground related housing;*
- f. *Low-rise apartment buildings; and,*
- g. *Communal housing*
- v. *In addition to permitted residential uses, lands designated Medium Density Residential may permit the following uses, subject to applicable policies of the Township of West Lincoln Official Plan and an amendment the Zoning By-law, where applicable*
  - a. *Home Occupations;*
  - b. *Public and Private Utilities;*
  - c. *Bed and Breakfast Establishments;*
  - d. *Day Care Facilities;*
  - e. *Places of Worship;*
  - f. *Educational Facilities;*
  - g. *Community Facilities;*
  - h. *Accessory Apartments; and,*
  - i. *Garden Suites*
- vi. *Medium Density Residential uses shall have a maximum density of 45 units per hectare*
- vii. *The maximum building height in the Medium Density Residential designation shall be 4 storeys*
- viii. *The Township may consider a maximum height of 5 storeys in the Medium Density Residential designation, on a site specific basis, provided the increased building height is compatible with the built form and physical character of the neighbourhood and complies with the Township's urban design guidelines*
- ix. *Medium Density Residential development shall be subject to Site Plan Control and shall be designed and located consistent with the direction of the Township's Urban Design Manual*

**Planning Comment:** Within the Draft Plan, Lots 46 -143, 157-177 and 178-205 are lotted for medium density residential uses, in the form of two and three-storey, back-to-back and bungalow townhouse dwellings. The conditions of Draft Plan of Subdivision and the Draft Plan of Common Element Condominium will allow for the same review and details that Site Plan Control would require. The maximum density of the proposed development will be 34 units per hectare, therefore conforming to the maximum density of 45 units per hectare.

- e) *Commercial*
  - i. *The planned function of the Commercial designation is to accommodate commercial development that will serve the local neighbourhood. It is the intent of Commercial area to act as a connecting centre for the neighbourhood and create opportunities for residents to obtain commercial goods and services, required to meet their day to day needs, within a reasonable walking and cycling distance. Accordingly, a range of retail and convenience commercial uses, business and professional offices that would serve the surrounding neighbourhood are appropriate.*

- ii. *Commercial uses should be small scale in nature and developed up to a maximum of 2,000 square metres of gross leasable area.*
- iii. *Lands designated Commercial may be zoned to permit the following uses:*
  - a. *Mixed use and freestanding commercial buildings, which may include residential uses above the first storey;*
  - b. *Office;*
  - c. *Clinic;*
  - d. *Retail;*
  - e. *Personal service;*
  - f. *Commercial service; and,*
  - g. *Restaurant.*
- iv. *The Township may permit freestanding residential buildings within the Commercial designation.*
- v. *Notwithstanding the above, low density residential uses are not permitted within the Commercial designation.*
- vi. *The Township may permit Drive Through Restaurants within the Commercial designation.*
- vii. *The following uses are prohibited within the Commercial designation*
  - a. *Automotive-oriented uses of any kind (sales, service, gas stations); and,*
  - b. *Adult entertainment establishments*
- viii. *The Zoning By-law may further refine the list of permitted and prohibited uses.*
- ix. *Buildings shall have a minimum building height the equivalent of 2 storeys and a maximum building height of 5 storeys.*
- x. *The Township may consider a maximum height of 6 storeys in the Commercial designation subject to the submission of a design brief which demonstrates the proposed increase in height will maintain an appropriate public realm and pedestrian streetscape.*
- xi. *For commercial developments which propose a residential component, the density requirements of the High Density designation shall be applied to the residential portion.*
- xii. *Commercial facilities shall be designed to provide a safe environment for pedestrians and encourage walking.*
- xiii. *Buildings shall be located such that they help to frame the street and vehicle parking is located to minimize conflicts with pedestrians.*
- xiv. *Surface parking lots are to be adequately screened.*
- xv. *Lighting shall be oriented away from residential areas and have minimal impact on existing and new residential uses.*
- xvi. *Development within the Commercial designation shall be subject to Site Plan Control and shall be designed and located consistent with the direction of the Township's Urban Design Manual.*
- xvii. *A design brief in support of site plan applications for commercial development is required.*
- xviii. *Special studies, as deemed appropriate by the Township and the Region, may be required at the site plan approval stage to ensure compatibility of non-residential uses with residential and other sensitive uses.*

- xix. *Existing driveways will be relocated to new local streets if possible to avoid conflicts between existing entrances and new street New entrances and driveways along the frontage of Regional Road 20 shall only be permitted subject to the approval of the Regional Municipality of Niagara*

**Planning Comment:** Block 208 within the proposed Draft Plan has been designated for the proposed three-storey mixed-use residential commercial building. Residential uses will be located on Floors two and three, while commercial/retail uses will be at-grade. A maximum of 1,303 square metres of commercial space will be provided, in compliance with the maximum requirement of 2,000 square metres identified above. The design of the proposed commercial area is anticipated to function as a neighbourhood commercial area, serving residents in the nearby neighbourhoods. It has been designed to provide an active streetscape along Regional Road 20, to further activate the public realm to support the commercial uses.

As stated above, the minimum density requirements of 40 units per hectare for High Density Residential are applied for the residential component. A maximum of 58 units per hectare will be provided. Stated throughout this report, the proposed residential component of the mixed-use building will compensate for the loss of the planned high-density residential, therefore the proposed density is appropriate at this location.

- h) *Parks*
- i. *The planned function of the Parks land use designation is to provide the Northwest Quadrant and surrounding neighbourhoods with convenient access to a broad range of uses, including municipal parks and other green spaces.*
- ii. *A park and trail system is a key component of the Secondary Plan. It is the intent that the park designation be developed consistent with the Neighbourhood Parks and Playgrounds and Linear Parks and Greenspace Corridors typologies identified in Section 9 of the Official Plan.*
- iii. *The amount and type of parkland within the community shall be consistent with the policies contained within Section 9 of the Township of West Lincoln Official Plan and will be determined through the draft plan of subdivision process.*
- iv. *Cash-in-lieu of parkland may be accepted by the Township in accordance with Section 9 of the Township of West Lincoln Official Plan and Section 37 of the Planning Act.*
- v. *Park blocks shall have a trail connection to promote connectivity via onstreet or off-street trails, which connect blocks, open spaces and other points of interest including the future Multi-Use Recreational Site.*
- vi. *Parks and trails shall be strategically located and linked to the broader trail system, as identified in the Township's Trails and Corridors Master Plan.*
- vii. *All trails shall be designed consistent with the Township of West Lincoln Trails and Corridors Master Plan.*
- viii. *Parks shall be easily accessible to residents and will be located within close proximity of all residential areas.*
- ix. *The Township encourages the integration of stormwater management facilities as part of the parks and open space system. Wherever feasible and appropriate, such lands shall be designed to provide a park-like setting or the character of a natural wetland and, where appropriate, may be adjacent to public parks. However, stormwater management facilities shall not be accepted as parkland under the parkland dedication provisions of the Township Official Plan and the Planning Act.*

- x. *Lands designated for park use adjacent to the rail lines will provide noise and safety buffers for adjacent residential uses. Consultation with the appropriate rail company, and completion of appropriate studies (i.e. noise and vibration studies) will be required to determine specific noise and safety measures.*
- xi. *The planting of trees within parkland blocks is encouraged to provide shade and to enhance the urban forest*
- xii. *In addition to the provision of open space and parks, private amenity recreation space should also be provided through future development applications, particularly for lands designated for medium density residential uses.*
- xiii. *Minor adjustments may be made to the size and location of parks and open space through the development approval process, without further amendment to the Secondary Plan, as long as the general intent of the Secondary Plan is respected.*

**Planning Comment:** The northern portion of the subject lands will include trail and open space uses, which will provide a noise and safety buffer to the proposed residential uses.

i) *Stormwater Management*

i. *This Plan provides a general stormwater management strategy as follows:*

- a. *The lands located to the east of Grimsby Road 5 drain to the south east and stormwater management can be accommodated through an appropriate stormwater management facility that outlets to the south side of the railway tracks into an existing culvert*
- b. *The lands to the west of Grimsby Road 5 outlet to the south side of Regional Road 20 into the existing stormwater system, which outlets to Twenty Mile Creek*
- ii. *The location and size of stormwater management facilities is based on the general stormwater strategy and is approximate. A detailed stormwater management study will be required to be submitted, for consideration by the Township, the NPCA and Niagara Region as part a complete application for development.*
- iii. *A small portion of the Secondary Plan at the western edge currently drains to a small tributary of 20 Mile Creek that flows westerly beyond the Secondary Plan Area at the location of the potential Grimsby Road 6 extension. The portion of the tributary in the Secondary Plan area shall be maintained by:*
  - a. *Ensuring that an appropriate level of overland flow to this tributary is maintained in order to ensure no negative impact to the tributary;*
  - b. *Ensuring that the extension of Grimsby Road 6 includes an appropriate crossing for this tributary in order to ensure no negative impact to the tributary; and,*
  - c. *Requiring that an overall storm drainage plan be developed as part of development applications which addresses storm water quality and quantity controls.*

**Planning Comment:** The proposed storm-water management pond will continue to outlet to the south side of Regional Road 20 into the existing stormwater management system, which outlets to Twenty Mile Creek. The location of the stormwater management pond will be situated at the lowest point of the subject lands. The FSR, prepared by AJ Clarke identify that the size of the proposed pond had to be increased to accommodate anticipated growth. As such, the planned High Density Residential units at the southern portion of the subject lands were reallocated to more medium density and mixed-use apartment units.

#### **6.11.5.4 DEVELOPMENT PHASING**

*The purpose of the Secondary Plan is to ensure the orderly and logical development of the Northwest Quadrant lands. Draft Plans of Subdivision shall consider the following policies:*

- i. Full development of the lands will require the installation of water and sanitary sewer infrastructure*
- ii. This plan provides a general servicing strategy as follows:*
  - a. Lands located on the west side of Grimsby Road 5 will generally be serviced through the extension of existing services located on Highway 20.*
  - b. Lands located on the east side of Grimsby Road 5 will generally be serviced by an extension of the servicing system in the subdivision located to the east and will be connected at Las Road.*
- iii. Plans and functional servicing reports prepared in support of plans of subdivision are to identify staging and the rationale in support of the proposed staging.*

**Planning Comment:** The proposed development will be serviced by an existing watermain along South Grimsby Road 5 and the Streamside Pumping Station. The necessary upgrades will be conducted to ensure the full optimization of existing municipal services. Please refer to the FSR for more details.

**Northwest Quadrant Secondary Plan Conclusion:** The Draft Plan of Subdivision conforms the Northwest Quadrant Secondary Plan, as it adheres to land use designations contained within the Land Use Schedule. The Secondary Plan allows some flexibility of the Plan, as adjustments can be made to the size and location of the land use designations. The high-density residential designation was removed as it was not contextually appropriate with the surrounding built form and character, in addition the size of the proposed storm-water management pond was increased to accommodate anticipated growth.

Though the high-density residential area was removed from the southern portion of the subject lands, the units were redistributed to a larger area of medium density residential and apartment units within the Commercial designation. The density requirement for the mixed-use residential component are in line with the minimum density requirement of 40 units per hectare of High Density Residential.

The proposed Land use Schedule of the proposed development is similar to the Secondary Plan, except for the placement of Street F and the low density designation.

## 8.7 Township of West Lincoln Zoning By-law 2017-70

The Township of West Lincoln Comprehensive Zoning By-law No. 2017-70 was passed by the Council of the Township of West Lincoln on June 26, 2017. It regulates the use of the lands, frontage and lot area of a parcel of land, the proportion of land occupied by a building, structure or storage, and the amount of landscaping. Zoning By-law No. 2017-70 replaces Zoning By-law 79-14 and implements policies of the Township of West Lincoln Official Plan.

As seen in the figure below, the subject lands are currently zoned D 'Development'. This zone permits Conservation Uses, Public Parks for passive recreation and a single detached dwelling on an existing lot of record.



Figure 8-13: Excerpt of Schedule S1 of the Township of West Lincoln Zoning By-law

**Township of West Lincoln Zoning By-law Conclusion:** The proposed development is not a permitted use within the 'D' Zone, therefore a Zoning By-law Amendment is required to implement the appropriate dwelling typology and regulations.

## 9 Planning Instruments

### 9.1 Proposed Zoning By-law Amendment

Under Section 34 of the Planning Act, a ZBLA may be used to amend any by-law passed under that section. Specifically, the proposed ZBLA is required to amend Schedule 'S1' of the Township of West Lincoln Zoning By-law, by rezoning the lands from the 'Development D' Zone to the Residential Low Density – Type 3 (R3-\_) Modified Zone, Residential Medium Density – Type 3 (RM3-\_) Modified Zones, Service Commercial (C3-\_) Modified Zone and Open Space (OS) Zone.

To implement the various townhouse dwelling typologies, the proposed ZBLA will include three different modified RM3 zones.

The proposed ZBLA will also include provisions relating to minimum width of a traffic lane for driveways for residential uses and minimum setback for a driveway for a residential use with five or more dwelling units. These site-specific regulations are a result of the lotting pattern of the Draft Plan of Subdivision.

### 9.1.1 Residential Low Density – Type 3 (R3-X) Modified Zone

The proposed single detached dwellings will be rezoned to the R3 Zone. It is being requested that the Residential Low Density –Type 3 (R3) Zone be modified to recognize site specific provisions relating to minimum lot area and minimum lot frontage of a corner lot for a single detached dwelling. The proposed modification is shown below.

**Table 9-1:** R3-X Provisions

REGULATIONS			R3 ZONE	R3-X
Permitted Uses			<ul style="list-style-type: none"> <li>Single detached dwellings</li> <li>Semi-detached dwellings</li> </ul>	<ul style="list-style-type: none"> <li>Single detached dwellings</li> </ul>
Minimum Lot Area (Municipal Services)			300 square metres	290 square metres
Minimum Lot Frontage (Municipal Services)	Corner Lot	Single Detached Dwelling	12.5 metres	12 metres
	Other Lots	Single Detached Dwelling	10 metres	10.0 metres
Minimum Front Yard			4.5 metres	As per R3 Zone
Minimum Exterior Side Yard			3 metres	As per R3 Zone
Minimum Interior Side Yard			1.2 metres	As per R3 Zone
Minimum Rear Yard			7.5 metres	As per R3 Zone
Maximum Lot Coverage (Municipal Services)			45%	As per R3 Zone
Maximum Height			10 metres	As per R3 Zone
Minimum Landscaped Open Space			25%	As per R3 Zone

#### 9.1.1.1 Minimum Lot Frontage and Lot Area

The modification to the minimum lot area and frontage for a corner lot of a single detached dwelling is a slight decrease, due to lotting of the Draft Plan of Subdivision and the proposed municipal roads and sidewalks.

### 9.1.1 Residential Medium Density – Type 3 (RM3-X) Modified Zone (Street Townhouses)

With regards to the RM3 provisions for two and three storey townhouse units, it is being requested to address site-specific provisions pertaining to: minimum lot area, minimum lot frontage, minimum interior side yard, and minimum amenity area. The proposed modifications are shown below.

**Table 9-2:** RM3-X Provisions

REGULATIONS		RM3 ZONE	RM3-X
Permitted Uses		<ul style="list-style-type: none"> <li>• Apartment dwellings</li> <li>• Fourplex dwellings</li> <li>• Semi-detached dwellings</li> <li>• Stacked townhouse dwellings</li> <li>• Street townhouse dwellings</li> <li>• Townhouse dwellings</li> <li>• Triplex dwellings</li> </ul>	<ul style="list-style-type: none"> <li>• Street townhouse dwellings</li> </ul>
Minimum Lot Area (per dwelling unit)	Street Townhouse Dwellings	180.0 square metres	125 square metres
Minimum Lot Frontage	Street Townhouse Dwellings	6.0 metres/ unit	5.1 metres/ unit
Minimum Front Yard	Dwelling	4.5 metres	4.7 metres
	Private Garage	6.0 metres	6.0 metres
Minimum Exterior Side Yard		3.0 metres	4.6 metres
Minimum Interior Side Yard	Adjoining a lot in a low density residential zone	3 metres	1.0 metres
	Adjoining a lot in any other zone	1.2 metres	2.4 metres
Minimum Rear Yard	Adjoining a lot in a low density residential zone	7.5 metres	7.0 metres
	Adjoining a lot in any other zone	6.0 metres	8.7 metres
Maximum Lot Coverage (Municipal Services)		50%	45%

Minimum Separation Distance between dwellings on the same lot	Between exterior side walls	3 metres	4.8 metres
	Between exterior front or rear walls	12 metres	N/A
	Between exterior front or rear walls and side walls	7.5 metres	N/A
Maximum Height		12 metres	12 metres
Minimum Landscaped Open Space		25%	37%
Minimum Amenity Area	Dwellings with 3 or 4 dwellings units on one lot	20 square metres per dwelling unit	44.9 square metres
	Dwellings with 5 to 8 dwelling units on one lot	40 square metres + 10 square metres per dwelling unit	44.9 square metres

**9.1.1.1 Minimum Lot Area and Frontage**

Overall, the reduced minimum lot area for street townhouse dwellings is to facilitate a more compact built form.

**9.1.1.2 Minimum Interior Side Yard**

The decrease in the minimum interior side yard is in relation to the three storey townhouse block adjacent to the single detached lot at the corner of Street G and Street H. The intent of a side yard setback is to maintain compatibility between surrounding uses and setbacks from neighbours. There are no concerns relating compatibility, as both of the uses are residential in nature. Furthermore, the single detached lot's rear yard provide a sufficient setback from the townhouse block.

**9.1.1.3 Minimum Rear Yard**

The proposed modification is a slight decrease and does not result in any compatibility issues as all of the uses are residential in nature, and the built form has been designed to ensure that compatibility is not compromised.

**9.1.1.4 Minimum Amenity Area**

This provision does not apply for street townhouses as the amenity space provided to the rear of the building will be shared by the block. The amenity area per dwelling unit will not be specifically dedicated to the dwelling units.

### 9.1.2 Residential Medium Density – Type 3 (RM3-XX) Modified Zone (Bungalow Townhouses)

A Street Townhouse is defined as a townhouse dwelling, where each dwelling unit is located on a separate lot. Therefore, the bungalow townhouse units are a permitted use within the Residential Medium Density – Type 3 Zone.

For the bungalow townhouse units, it is being requested that the RM3 Zone be modified to address site-specific provisions relating to: minimum lot area per dwelling unit and minimum rear yard.

The proposed modifications are shown in the table below.

**Table 9-3:** RM3-XX Provisions

REGULATIONS		RM3 ZONE	RM3-XX
Permitted Uses		<ul style="list-style-type: none"> <li>• Apartment dwellings</li> <li>• Fourplex dwellings</li> <li>• Semi-detached dwellings</li> <li>• Stacked townhouse dwellings</li> <li>• Street townhouse dwellings</li> <li>• Townhouse dwellings</li> <li>• Triplex dwellings</li> </ul>	<ul style="list-style-type: none"> <li>• Street townhouse dwellings</li> </ul>
Minimum Lot Area (per dwelling unit)	Street Townhouse Dwellings	180.0 square metres	155 square metres
Minimum Lot Frontage	Street Townhouse Dwellings	6.0 metres/ unit	6.0 metres/ unit
Minimum Front Yard	Dwelling	4.5 metres	6.0 metres
	Private Garage	6.0 metres	6.0 metres
Minimum Exterior Side Yard		3.0 metres	3.0 metres
Minimum Interior Side Yard	Adjoining a lot in a low density residential zone	3 metres	3.3 metres
	Adjoining a lot in any other zone	1.2 metres	2.0 metres

Minimum Rear Yard	Adjoining a lot in a low density residential zone	7.5 metres	6.9 metres
	Adjoining a lot in any other zone	6.0 metres	7.0 metres
Maximum Lot Coverage (Municipal Services)		50%	50%
Minimum Separation Distance between dwellings on the same lot	Between exterior side walls	3 metres	4.4 metres
	Between exterior front or rear walls	12 metres	14.0 metres
	Between exterior front or rear walls and side walls	7.5 metres	9.0 metres
Maximum Height		12 metres	12 metres
Minimum Landscaped Open Space		25%	35%
Minimum Amenity Area	Dwellings with 3 or 4 dwellings units on one lot	20 square metres per dwelling unit	60 square metres
	Dwellings with 5 to 8 dwelling units on one lot	40 square metres + 10 square metres per dwelling unit	60 square metres

**9.1.2.1 Minimum Lot Area**

The reduced minimum lot area for bungalow townhouse dwellings is to facilitate a more compact built form.

**9.1.2.2 Minimum Rear Yard adjoining a Lot in a Low Density Residential Zone**

The decrease in the minimum rear yard setback is minor and in relation to a corner dwelling unit abutting the single detached lot at the corner of Street G and the private road. The intent of a rear yard setback is to provide suitable amenity space within a lot and maintain compatibility between surrounding uses and setbacks from neighbours. Sufficient amenity space is provided for the bungalow townhouse units and there are no concerns relating compatibility, as both of the uses are residential in nature. Furthermore, the single detached lot's rear yard provide a sufficient setback from the townhouse block.

### 9.1.3 Residential Medium Density – Type 3 (RM3-XXX) Modified Zone (Back-to-Back Townhouses)

In the case of back-to-back townhouse units, the proposed ZBLA will include the typology as a permitted use and will request site-specific provisions pertaining to: minimum lot area per dwelling unit, minimum rear yard, maximum lot coverage, minimum landscaped open space and minimum amenity area.

The proposed modifications are shown in the table below.

**Table 9-4:** RM3-XXX Provisions

REGULATIONS		RM3 ZONE	RM3-XXX
Permitted Uses		<ul style="list-style-type: none"> <li>• Apartment dwellings</li> <li>• Fourplex dwellings</li> <li>• Semi-detached dwellings</li> <li>• Stacked townhouse dwellings</li> <li>• Street townhouse dwellings</li> <li>• Townhouse dwellings</li> <li>• Triplex dwellings</li> </ul>	<ul style="list-style-type: none"> <li>• Back-to-back townhouse dwellings</li> </ul>
Minimum Lot Area (per dwelling unit)		180.0 square metres	95 square metres
Minimum Lot Frontage		6.0 metres/ unit	6.3 metres/ unit
Minimum Front Yard	Dwelling	4.5 metres	5.7 metres
	Private Garage	6.0 metres	8.4 metres
Minimum Exterior Side Yard		3.0 metres	4.0 metres
Minimum Interior Side Yard	Adjoining a lot in a low density residential zone	3 metres	N/A
	Adjoining a lot in any other zone	1.2 metres	4.0 metres
Minimum Rear Yard	Adjoining a lot in a low density residential zone	7.5 metres	N/A metres

	Adjoining a lot in any other zone	6.0 metres	0.0 metres
Maximum Lot Coverage (Municipal Services)		50%	58%
Minimum Separation Distance between dwellings on the same lot	Between exterior side walls	3 metres	4.0 metres
	Between exterior front or rear walls	12 metres	N/A
	Between exterior front or rear walls and side walls	7.5 metres	N/A
Maximum Height		12 metres	12 metres
Minimum Landscaped Open Space		25%	15%
Minimum Amenity Area	Dwellings with 3 or 4 dwellings units on one lot	20 square metres per dwelling unit	N/A
	Dwellings with 5 to 8 dwelling units on one lot	40 square metres + 10 square metres per dwelling unit	N/A
	Dwellings with 9 or more dwelling units on one lot	80 square metres + 5.5 square metres per dwelling unit	35 square metres

**9.1.3.1 Minimum Lot Area, Maximum Lot Coverage and Minimum Landscaped Open Space**

The modification to these provisions for back-to-back townhouse dwellings is due to the absence of individual rear yards. Rear-yard are typically include within the landscaped open space, however the proposed back-to-back built form will encompass more of the lot area.

**9.1.3.2 Minimum Rear Yard and Minimum Amenity Area**

The intent of a rear yard setback is to allow for the provision of suitable amenity space within a lot. The proposed back-to-back units do not include rear yards, and the loss of amenity space will be compensated through balconies and public amenity space within the proposed open space provided in the northern portions of the subject lands.

### 9.1.4 Service Commercial (C3-X) Modified Zone

The proposed commercial block will be rezoned to the Service Commercial ‘C3’, which permits a variety of commercial, retail and office uses. To allow for the proposed residential units on the second and third floors of the mixed-use building, the proposed ZBLA will add apartment residential units as a site-specific permitted use. It is being requested to include site-specific provisions relating to the minimum front yard and reduced parking standards.

The proposed modifications are shown in the table below.

**Table 9-5:** C3-X Provisions

REGULATIONS	C3 ZONE	C3-X
Permitted Uses	<ul style="list-style-type: none"> <li>• Commercial Kennel</li> <li>• Commercial school</li> <li>• Communications establishment</li> <li>• Drive-through facility</li> <li>• Dry cleaning/ laundry depot</li> <li>• Financial institution</li> <li>• Funeral Home</li> <li>• Garden Centre</li> <li>• Hotel</li> <li>• Motor Vehicle Dealership, Gasoline Bar, Repair Establishment, Service Station and Washing Establishment</li> <li>• Office, including medical office</li> <li>• Personal service shop</li> <li>• Pet care</li> <li>• Place of entertainment</li> <li>• Private club</li> <li>• Recreation facility</li> </ul>	<ul style="list-style-type: none"> <li>• In addition to permitted uses, apartment dwelling</li> </ul>

		<ul style="list-style-type: none"> <li>• Restaurant</li> <li>• Retail store</li> <li>• Service ship</li> <li>• Studio</li> <li>• Veterinary clinic</li> </ul>	
Minimum Lot Area (per dwelling unit)		750 square metres	103.5 square metres
Minimum Lot Frontage		25 metres	71.5 metres
Minimum Front Yard	Adjoining Regional Road 14 or 20	9 metres	3.0 metres
	Other	9.0 metres	N/A
Minimum Exterior Side Yard	Adjoining Regional Road 14 or 20	6.0 metres	N/A
	Other	6.0 metres	14.7 metres
Minimum Interior Side Yard	Adjoining a lot in a residential zone	9.0 metres	41.8 metres
	Other	6.0 metres	11.5 metres
Minimum Rear Yard	Adjoining a lot in a residential zone	9.0 metres	26.7 metres
	Other	6.0 metres	N/A
Maximum Lot Coverage		50%	23.8%
Maximum Height		10 metres	10 metres
Minimum Landscaped Open Space		10%	36%
Maximum Outside Storage		5% of Lot Area	5% of Lot Area
Maximum Gross Leasable Floor Area	Per Commercial Use	N/A	N/A
	Total % of Lot Area	50%	N/A
Parking	Office	1 space per 28 square metres (47 spaces)	42 spaces
	Apartment	1.5 spaces per unit (42 spaces)	

#### **9.1.4.1 Minimum Front Yard**

A reduced front yard will bring the proposed mixed-use building closer to the street, creating an active streetscape and improving the public realm in an effort to support at-grade commercial uses.

#### **9.1.4.2 Reduced Parking**

Approximately 42 spaces will be provided for the proposed mixed-use building, as the spaces are intended to be shared between the commercial and residential uses. It is anticipated that the apartment parking spaces will be vacant during the day and available for customers accessing commercial uses. Similarly, the commercial uses will close at night, leaving the spaces available for residents.

#### **9.1.5 Open Space (OS) Zone**

The permitted uses within the Open Space (OS) Zone include a private/public park. The proposed stormwater management pond is also included within this zone. There are no modifications proposed to the regulations within this zone.

Please refer to **Appendix F** for the proposed Zoning By-law Amendment.

## **10 Planning Justification**

The following section of the Planning Justification Report provide land use planning rationale with regard to the proposed applications.

### **10.1 Public Interest**

Registered Professional Planners (“Planners”) have a responsibility to acknowledge the interrelated nature of planning decisions and the consequences for natural and human environments, and the broader public interest. The public interest reflects a balance between the local needs of the community with the interests of stakeholders. In order to determine whether the proposed development is within the public interest, the balance of the elements of sustainable development have to be evaluated, including environmental, economic and social aspects. This is in addition to complying with public interest through planning and regulatory framework.

#### **10.1.1 Environment**

As demonstrated throughout this report, the existing vegetation on the subject lands do not constitute as a provincially significant woodland, confirmed by a preliminary site visit by Colville Consulting. Therefore, the proposed development effectively protects natural heritage features, by increasing residential and commercial density on vacant DGA lands compared to developing upon lands that may have significant environmental features.

#### **10.1.2 Economy**

By promoting efficient land development patterns, the proposed development will decrease the need for uneconomical land and servicing expansions, thereby sustaining the financial wellbeing of the province, Region and municipality.

The proposed development of 224 residential units will provide financial contribution to the City and Region, through taxes and development charges. Though preliminary, the proposed mixed-

use commercial block will also diversify the economic base of Smithville and provide employment opportunities.

### 10.1.3 Social

The Planning Act identifies promoting a mixture of residential units as matter of provincial interest, while the PPS requires that a mixture of residential unit types and densities be provided to meet projected requirements of current and future residents.

The proposed development does not conflict with the Growth Plan, as the addition of 224 residential units will contribute towards accommodating the population forecasts defined in the Growth Plan. It also conforms to one of the guiding principles of the Growth Plan, which is to support a range and mix of housing options.

As seen in the unit mix of the Township, the majority of the dwellings within the Town of West Lincoln are in the form of low density. The proposed development will provide a variety of different forms of housing, including 147 residential units in the form of medium density and a maximum of 32 mixed-use residential apartment units. By addressing the housing needs for a variety of household sizes, incomes and ages, the proposed development supports the long-term viability and well-being of the Township of West Lincoln.

## 11 Conclusion and Recommendations

Marz Homes (Smithville West) Inc. is proposing the development (“Thrive”) of approximately 224 residential units, comprised of a variety of dwelling types, including apartment, single detached, bungalow townhouse, back-to-back, three storey and two-storey townhouse units. It will also include a mixed-use commercial building, consisting of a maximum of 1,303 square metres of commercial space and two storeys of residential uses, and blocks, intended for a storm-water management pond, parkland/open space and trails. The proposed development will accommodate the residential needs and growth of the Smithville Urban Area, while diversifying housing stock to respond to changing demographics.

In order to facilitate the development, a Zoning By-law Amendment (“ZBLA”) is required to rezone a portion of the subject lands from the Development ‘D’ Zone to three Residential Medium Density – Type 3 ‘RM3’ Modified Zones and a Residential Low Density ‘R3’ Zone. A ZBLA is required to rezone the mixed-use commercial building to Service Commercial ‘C3’ Modified and open space uses to the Open Space “OS” Zone. A Draft Plan of Subdivision is also required to subdivide the property, while a Draft Plan of Common Element Condominium will be submitted, in the future, to share ownership of a portion of the development.

The proposed development and subsequent planning applications:

- Consistent with Provincial Policy Statement 2020, as it accommodates an appropriate range of land uses and mix of dwelling typologies to accommodate residential and employment growth;
- Conform to the 2019 Growth Plan, as the proposed development meets applicable Greenfield Density targets;
- Conforms to the Region of Niagara Official Plan, as it facilitates orderly development to adjacent parcels within the Smithville Urban Area;
- Conforms to the Township of West Lincoln Official Plan, as it develops vacant DGA lands into a compact neighbourhood with a variety of uses

- Maintains the intent of the North-west Quadrant Secondary Plan, which allows some flexibility within the Land Use Plan;
- Is compatible with the surrounding community, through designating 'Future Development' to ensure compatibility with adjacent parcels; and,
- Represents good planning and is in the public interest.

Based on a review of the subject lands, the surrounding lands, supporting studies, and the applicable planning policy framework, the subject applications represent good planning and facilitate an appropriate form of development in the Township of West Lincoln.

Respectfully submitted this 16th day of July, 2020.

Regards,

**IBI Group**



Angela Buonamici MCIP, RPP  
Senior Planner



Ritee Haider BES  
Planner

# Appendix A – Preliminary Concept Plan

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# Appendix B – Preliminary Site Plan

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# Appendix C – Draft Plan of Subdivision

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# Appendix D – Draft Plan of Common Element Condominium

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# Appendix E – Pre-Consultation Meeting Form

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## Pre-consultation Meeting Form

Persons intending to make an application for a proposed development are required to consult with planning staff prior to submitting an application. A pre-consultation meeting will identify what is required to be submitted for a complete application and will provide the opportunity to discuss:

- the nature of the application;
- development and planning issues;
- fees;
- the need for information and/or reports to be submitted with the application;
- the planning approval process;
- other matters, as determined.

**Individuals who make written submissions with respect to a Planning Act application should be aware that their submission and any personal information in their correspondence will become part of the public record and made available to the Applicant, Committee and Council.**

**Pre-Consultation Meeting Date:** APRIL 4, 2019

**Site Address:** RN:260203001231800000 **Approximate Land Area (metric):** 10.35 HECTARES

**Site Legal Description:** PLAN M98 LOT 9

**Owner Contact Information:**

Name of Owner: MARZ HOMES (SMITHVILLE WEST) INC

Phone Number: \_\_\_\_\_

Email: \_\_\_\_\_

Principal Contact: ANGELA BUONAMICI – IBI GROUP

Phone Number: 905-546-1010 ext 63102

Email: [Angela.buonamici@ibigroup.com](mailto:Angela.buonamici@ibigroup.com)

**Application Type:**

<input type="checkbox"/> Regional Official Plan Amendment	<input type="checkbox"/> Draft Plan of Condominium	<input checked="" type="checkbox"/> Draft Plan of Subdivision
<input checked="" type="checkbox"/> Local Official Plan Amendment*	<input type="checkbox"/> Vacant Land & Common Element	<input type="checkbox"/> Other
<input checked="" type="checkbox"/> Zoning By-law Amendment	<input type="checkbox"/> Conversion	
<input type="checkbox"/> Consent (Land Severance)	<input type="checkbox"/> Other	
<input type="checkbox"/> Site Plan Approval		

\*See Section 5.

**Local Municipal Contact:** Gerrit Boerema **Phone:** 905-957-5133 **Email:** gboerema@westlincoln.ca

1. Brief description of proposed development:

The applicants wish to apply for a draft plan of subdivision and rezoning for the subject lands to allow for approximately 201 units (could include 52 singles, 28 bungalow towns, 28 back to back towns, 76 standard towns). The development is also proposed to have a SWM pond, open space, a commercial block located at the corner of RR 20 and S. Grimsby Road 5. The property is approx.. 25 acres and is currently zoned Development 'D'.

2. Check All Applicable:  Brownfield  Greenfield  Built-up  Greenbelt  Local CIP Area

3. Development Charges: Regional  Local

4. Existing Regional Policy Plan Designation: Urban (Designated Greenfield) Area – Smithville

Conformity with Regional Policy Plan land use designations and policies? : yes  no  unknown

If 'No', what is the nature of the amendment needed? \_\_\_\_\_

5. Existing Local Official Plan Designation: NORTHWEST QUADRANT SECONDARY PLAN AREA – SEE ATTACHED

Conformity with Official Plan land use designations and policies?  Yes  No

If 'No', what is the nature of the amendment needed? If application does not align with Secondary Plan a Official Plan amendment would be required. Planning Staff need to see more detail on how the proposal fits within the greater community. The Secondary Plan allows for flexibility as long as the intent of the plan is maintained to the satisfaction of the Township. IBI Group to provide justification on variations from the secondary plan.

6. Existing Zoning: Development 'D'

Conformity with existing zoning?:

Yes  No

If 'No', what is the proposed zoning: A zoning bylaw amendment will be required to rezone the lands zoned development. The zoning should generally reflect the Secondary Plan designations.

7. Urban Design Guideline Applicable?

Yes  No

8. Is Site Plan approval required?

Yes  No

Will be required for future condominiums.

9. Fees Required at time of Submission of the Application

Application	Township Planning Department	Region of Niagara	Niagara Peninsula Conservation Authority	Other Fees
Regional Policy Plan Amendment				
Local Official Plan Amendment				
Zoning By-law Amendment	\$6,535.00**	\$1,270.00	\$1,875.00	
Plan of subdivision	\$7,675.00**	\$5,075.00 +\$1,120.00 per hectare	\$570 - \$2,770.00	
Plan of Condominium	(separate fees when plans of condos come forward)			
Consent				
Site Plan Control or Amendment				
Other – Constraints Review Fee		\$1,100.00	\$1,015.00	
Other – SWM Review Fee		\$1,810.00		
Subdivision Agreement	\$7,075**			To be paid prior to assumption
<b>TOTAL</b>				

\*not required if the subdivision is submitted concurrently with the ZBA. Otherwise \$570.00 if the subdivision is less than 100 lots and \$2770.00 if the subdivision is greater than 100 lots.

\*\* Township fees to increase later this year. See attached Appendix A for full list of fees.

Notes on Fees:

- Notwithstanding the fees noted above, all fees are payable based upon the rate in the fee schedule by-law in effect on the date the application is received.
- Further fees may be required at a later date as per the fee schedule by-law.
- Separate cheques shall be made payable to the appropriate agency.
- In addition to all application fees set out above, the applicant shall pay to the Township prior to final approval, all peer review fees, engineering review costs and legal fees incurred by the Township in reviewing the application.

10. Development Charges – Contact Chief Building Official prior to Permits for rates

11. Additional Agencies to be contacted:

HYDRO  PIPELINES  CP RAIL  OTHER \_\_\_\_\_

12. Additional Comments:

**TOWNSHIP PLANNING** Refer to comments sent by the Township in the Memo dated December 7, 2018 as there does not appear to be many changes from the plan originally submitted. Planning justification will need to be provided by the applicants for why the layout and designations of the secondary plan cannot be applied in this development. The provided concept does not appear to show how the development corresponds to future development to the west or existing development to the east (road alignment of Westlea and Northridge). Please show this on the plan as well as how this development will still allow for orderly development of adjacent parcels of land. The draft plan needs to be planning on a

wider context and not just for the individual property. Township Planning staff will consult with CP and Public works to see if the Temporary access is permitted in its planned location or if it should be located farther south. Urbanization will be required along the properties frontage on South Grimsby Road 5. The Stormwater Pond should be designed to incorporate lands to the north. If this is not possible, Planning Staff will need to see justification why the secondary plan cannot be complied with. The SWM pond should be designed to meet urban design standards so that it is an attractive feature of the development as it is in a prominent location. An urban design report will be required as part of the subdivision application and a fully detailed report required as a condition of subdivision. A full trail and park design will also be required. The trailhead should incorporate designs including park benches, pergolas and landscaping. Provision should be made to allow for access through the condo blocks to have access to the 'rail trail'. If the applicants are proposing to rezone the commercial designated area, the proposed zoning will be limited to those identified in the Secondary Plan. Access points should be shown for the Commercial areas. A parking analysis will be required to show that there is sufficient parking available for the development. Controls will be used to ensure that other landowners contribute where they receive benefits from the infrastructure installed. Planning staff would like to see the phasing and implementation of this subdivision – we would like condominiums and free-holds to be developing at the same time. A minimum density within the Township OP is 50 people and jobs per hectare, however, the Township believes a target of 60 could be achievable on this property. The Township will look into examining if a constraints analysis is needed or not. We will review comments submitted by the NPCA and Region during the secondary plan and SWAP process.

### **REGION OF NIAGARA**

According to the ROP, a minimum combined gross density target of 50 people and jobs per hectare across all Designated Greenfield Areas is required. Township is to ensure conformity with Secondary Plan (Northwest Quadrant); property abuts active rail line. Unmapped environmental features on the property (two watercourses, treed areas in the northwest and southeast portions that exhibit wetland characteristics and may provide wildlife habitat) – Constraints Analysis is required to characterize these areas. Require daylight triangles (4.5m x 4.5m) at entrance along RR 20 (confirming if road widening is required); need Regional Construction Encroachment and Entrance Permit for access from RR20. Need MOECP applications for extension of municipal sewers, require Functional Servicing Report to identify servicing strategy including the greater development area, identify if any upgrades would be needed to the SPS to accommodate the development. Require SWM report, runoff should be captured and treated to an Enhance standard prior to discharge from the site, need detailed grading, storm servicing, stormwater management, and construction sediment control drawings. Development will need to comply with Regional Waste Collection Policy, and ensure that the design of the roads allows for trucks to make necessary maneuvers on-site. See attached Sanitary Pumping Station Comments.

### **NPCA**

There is an area at the west end of the property that may contain unevaluated wetland (refer to attached map). The NPCA will require as part of this application, a constraints analysis to examine the feature and determine if it is in fact a wetland. The report will need to provide an impact analysis if the feature is determined to be a wetland. I understand the Region will be making a similar request. Our review fees for this would be as follows: ZBA: \$1875.00 (major); Constraints Review Fee (EIS): \$1015.00; Subdivision: not required if the subdivision is submitted concurrently with the ZBA. Otherwise \$570.00 if the subdivision is less than 100 lots and \$2770.00 if the subdivision is greater than 100 lots.

### **TOWNSHIP PUBLIC WORKS**

Will need to have the required studies and reports peer reviewed at the owner's expense. Please see the list of report requirements. Will require that south Grimsby Road 5 along the properties frontage be urbanized with sidewalk and lighting.

### **TOWNSHIP BUILDING**

The building department will need a plan to show how and where fill will be distributed on the site and how that affects building elevations. Development charges will apply. Building permits will be required. If there is cut and fill then an engineering report will be needed to ensure that the engineered fill is satisfactory to the building code. Need to ensure that fire flows are within acceptable ranges.

### **OTHER**

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13. **Site Visit:** \_\_\_\_\_

14. **Incentive Programs:**  
\_\_\_\_\_  
\_\_\_\_\_

15. Required Information and Studies to be submitted with the Application(s). Studies identified with an asterisk\* will likely require a peer review at the cost of the developer.

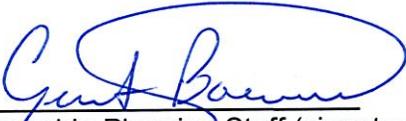
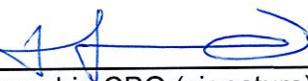
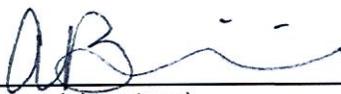
Local	Region	NPCA	Reports, Studies, Plans (See Notes for additional details)	No. of Copies		Notes
				Elect Digital	Paper	
✓	✓	✓	Planning Justification Report	✓	7	How it aligns with the secondary plan - Detailed
✓	✓	✓	Conceptual Site Plan and Subdivision Plan			
			Draft Regional Policy Plan Amendment			
✓	✓		Draft Zoning Bylaw Amendment	✓	7	Can be within PJR - Detailed
			Land Use/Market Needs*			
✓			Urban Design Report and Landscape Plans	✓	7	Needs to be as detailed as possible for initial submission, but full details required as condition of subdivision
✓	✓		Archaeology Assessment	✓	7	Can Resubmit what was submitted for Dunloe
			Cultural Heritage Impact Assessment*			
			Environmental Impact Study			
			Environmental Planning Study/ Sub-Watershed Study			
✓			Tree Inventory Preservation Plan	✓	7	TPP and Replacement Plan - Will be Condition of subdivision
			Floodplain and Hazard Lands Boundary Plan			
✓			Geotechnical	✓	7	
			Environmental Site Assessment			
✓	✓		Air Quality/Noise & Vibration Study*	✓	7	Needs to address adjacent commercial uses, rail, road, pumping station
			Agricultural Impact Assessment			
			Farm Operation and Ownership			
			Minimum Distance Separation I & II			
			Mineral Aggregate Resources			
✓	✓		Municipal Servicing Study (Functional Servicing)	✓	7	Include water for fire flows. High level at this stage – detailed as condition of sub.
			Phasing Plan			
			Sensitive Land Use Report (to be captured in noise study)			
			Slope Stability Report			
✓	✓	✓	Stormwater Management Plan	✓	7	High Level – Detailed Report as a condition of subdivision.
✓	✓		Transportation Impact Study/Parking Impact Analysis	✓	7	Full report
			Hydrogeological Study and Private Servicing Plans*			
✓			Soil report	✓	7	
			Financial Impact Assessment*			
			Shadow Analysis			
			Risk Management Study			
✓			Gas Well Study/Gas Migration Study	✓	7	Confirmation of no gas wells
			Wind Study*			
			Cistern for Fire Purposes under OBC			
✓	✓	✓	Other* - <b>Environmental Constraints Analysis</b>	✓	7	Subject to further consultation with NPCA and Region

**Notes:**

1. The purpose of this document is to identify the information required to commence processing and evaluating an application as set out in the Planning Act. This pre-consultation process is designed to proceed based on the mutual agreement of the parties as shown by the signatures below.
2. Pre-consultation does not imply or suggest any decision whatsoever on behalf of staff or the municipality to either support or refuse the application.
3. The applicant should be aware that the information provided is accurate as of the date of the preconsultation meeting. Should an application not be submitted in the near future, and should other policies, by-laws or procedures be approved by the Province, Municipality, Region or other agencies prior to the submission of a formal application, the applicant will be subject to any new policies, by-laws or procedures that are in effect at the time of the submission of a formal application. If an application is not submitted within 1 year, it is advisable that the applicant confirm with the municipality the directives of the original preconsultation meeting.

4. Any application submitted without the information identified in this Pre-consultation Document will be deemed incomplete and not processed. Alternately, staff may recommend refusal of the application based upon insufficient information to properly evaluate the application.
5. The applicant acknowledges that the Municipality and Region considers the application forms and all supporting materials including studies and drawings, filed with any application to be public information and to form part of the public record. With the filing of an application, the applicant consents and hereby confirms that the consent of the authors of all supporting reports have been obtained, to permit the Municipality and Region to release the application and any supporting materials either for its own use in processing the application, or at the request of a third party, without further notification to, or permission from, the applicant.
6. It is hereby understood that during the review of the application additional studies or information may be required as a result of issues arising during the processing of the application or the review of the submitted studies.
7. If the Municipality or Region does not have sufficient expertise to review and determine that a study is acceptable, the Municipality may require a peer review. The cost of the peer review shall be paid for by the applicant. The Terms of Reference for a peer review is determined by the Municipality or Region.
8. Some studies may require NPCA review and clearance/approval. In this instance, the NPCA review fee shall be paid by the applicant.
9. All plans and statistics must be submitted in metric.
10. It is hereby understood that during the review of the application additional applications, studies or information may be required as a result of issues arising during the processing of the application.
11. There may also be financial requirements arising from the application, including, but not limited to, parkland dedication, development charges, payment of outstanding property taxes, deferred local improvement charges, cost for lifting 0.3 metres reserves, and reimbursement for road widening acquisition or road improvements.
12. Engineering review done in association with an application will be billed to the applicant.

**Signatures:**

<u>Gerrit Boerema</u> Township Planning Staff	 Township Planning Staff (signature)	<u>April 4, 2019</u> Date
<u>Jennifer Bernard</u> Township Public Works Staff	 Township Public Works (signature)	<u>Apr 4/19</u> Date
<u>JEFF MENARD</u> Township Building Staff	 Township CBO (signature)	<u>Apr 4/19</u> Date
<u>Aimee Alderman</u> Regional Staff	 Regional Staff (signature)	<u>April 4, 2019</u> Date
<u>Jennifer Whittard</u> Regional Staff	 Regional Staff (signature)	<u>Apr. 4 '19</u> Date
_____ NPCA Staff	_____ Conservation Staff (signature)	_____ Date
<u>Angela Buonamici</u> Agent	 Agent (signature)	<u>Apr. 4 2019</u> Date
<u>Anthony Charcella</u> Owner	 Owner (signature)	<u>Apr 4/19</u> Date
_____ Other	_____ Other (signature)	_____ Date

## Gerrit Boerema

---

**From:** Alderman, Aimee <Aimee.Alderman@niagararegion.ca>  
**Sent:** Friday, April 05, 2019 2:23 PM  
**To:** Gerrit Boerema  
**Subject:** Streamside/Smithville SPS Information

Hi Gerrit,

In follow-up from the subdivision item at the Pre-Consultation meeting yesterday (IBI/Marz), please see the information below regarding the Streamside Sewage Pumping Station and Master Servicing Plan:

The proposed subdivision is outside the current drainage areas of both the Streamside SPS and the Smithville SPS, however, during the recently completed Master Servicing Plan (MSP) the expansion of the urban boundary area for WL was considered and significant growth was added to the Smithville SPS sewershed and some growth was added to the Streamside SPS in collaboration with the Township of West Lincoln:

- The current operational firm capacity of the Streamside SPS is 23.6 L/s and Smithville SPS is 120 L/sec. The MSP has identified the existing and future dry weather flows for the Streamside station had the capacity to accommodate the growth that was predicted – the Smithville SPS would require an upgrade
- The Smithville SPS sewershed does experience wet weather flows and the Township is working on an Infiltration and Inflow reduction program from the sewershed. Both current and future wet weather flow design calculations exceed the current capacity of the station.
- The Streamside SPS sewershed has the capacity to accommodate the current and future wet weather flow design calculated flow.
- There was a capital budget project carried in the MSP for first timeframe (2022-2031) for the twining of the forcemain from the Smithville SPS and upgrade of the SPS
- No projects were carried for the Streamside SPS since there was limited growth added to this sewershed
- The MSP can be found at the following link:  
<http://www.niagararegion.ca/2041/master-servicing-plan/default.aspx>

### **Aimee Alderman, MSc**

#### **Development Planner**

Planning and Development Services Department

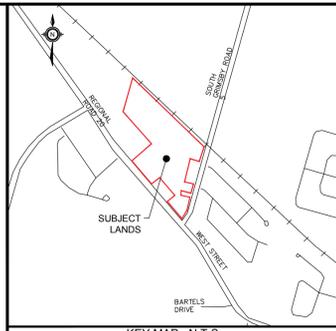
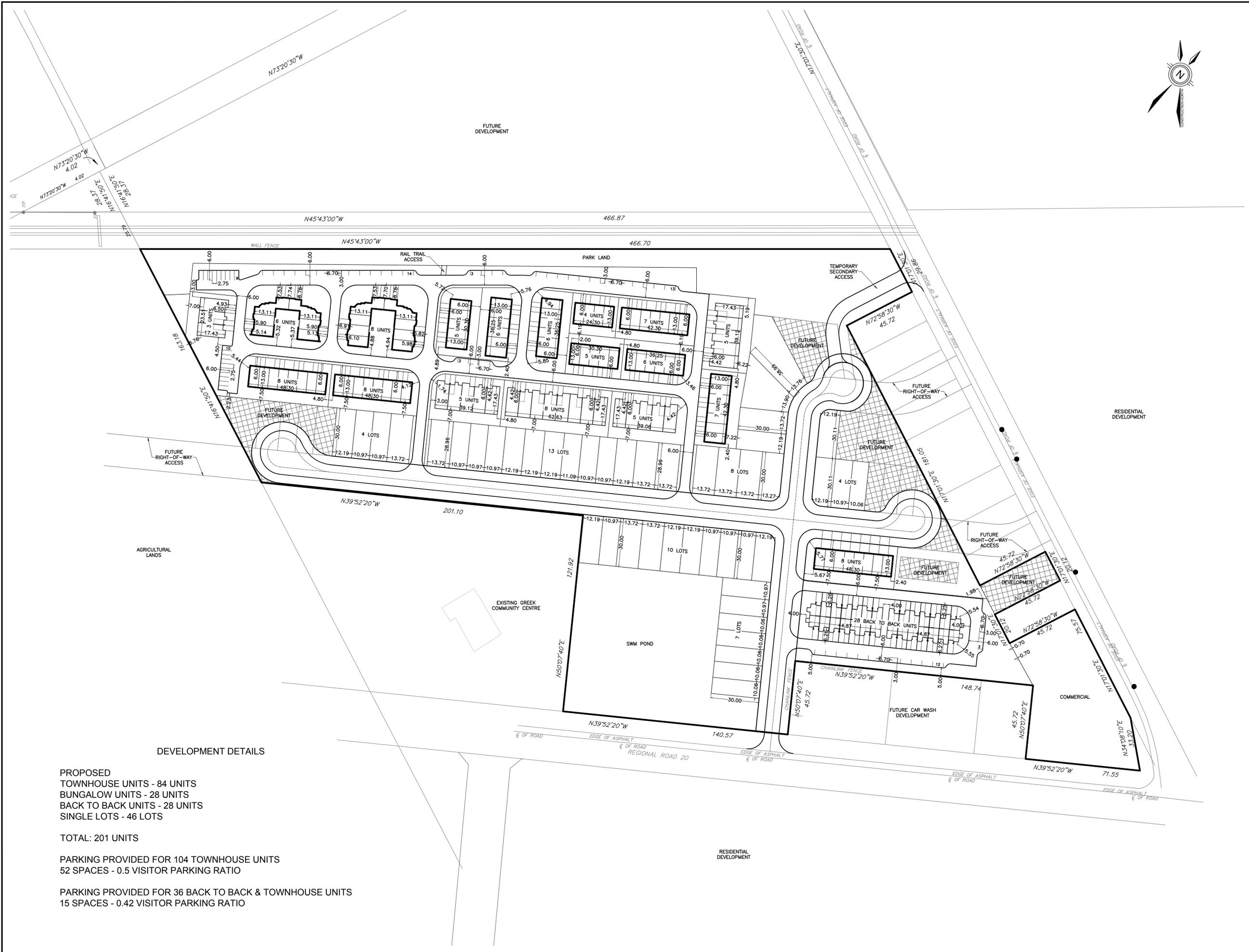
Niagara Region | [www.niagararegion.ca](http://www.niagararegion.ca)

1815 Sir Isaac Brock Way, Thorold, ON L2V 4T7

P: 905-980-6000 ext. 3352 Toll-free: 1-800-263-7215

E: [aimee.alderman@niagararegion.ca](mailto:aimee.alderman@niagararegion.ca)

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KEY MAP - N.T.S.

LEGEND

XXXX	SOURCE:
XXXX	BENCHMARK
SCALE	
DESIGN BY: A BUONAMICI	CHECKED BY: J ARIENS
DRAWN BY: S CHALMERS	DATE: 2019-02-25

**NOT FOR CONSTRUCTION**

NO	DATE	BY	DESCRIPTION

DRAWING ISSUE RECORD

NO	DATE	BY	DESCRIPTION

APPROVALS


**IBI GROUP**  
 200 East Wing-360 James Street North  
 Hamilton ON L8L 1H5 Canada  
 tel 905 546 1010 fax 905 546 1011  
 ibigroup.com

TOWNSHIP OF  
 WEST LINCOLN

URBAN EXPANSION BLOCK

SECONDARY PLAN CONCEPT

FILE NUMBER: 109178 SHEET NUMBER: SP1

**DEVELOPMENT DETAILS**

PROPOSED  
 TOWNHOUSE UNITS - 84 UNITS  
 BUNGALOW UNITS - 28 UNITS  
 BACK TO BACK UNITS - 28 UNITS  
 SINGLE LOTS - 46 LOTS

TOTAL: 201 UNITS

PARKING PROVIDED FOR 104 TOWNHOUSE UNITS  
 52 SPACES - 0.5 VISITOR PARKING RATIO

PARKING PROVIDED FOR 36 BACK TO BACK & TOWNHOUSE UNITS  
 15 SPACES - 0.42 VISITOR PARKING RATIO



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**PLANNING & DEVELOPMENT DEPARTMENT  
MEMORANDUM**

**TO: Anthony Chiarella**

**FROM: Gerrit Boerema & Brian Treble, Township of West Lincoln**

**DATE: December 7, 2018**

**SUBJECT: Draft Plan of Subdivision Preliminary Planning Review**

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The following comments should be considered as preliminary, based on the draft plan of subdivision sketch provided (Secondary Plan Concept 4b). Prior to the application being submitted, the applicants will need to attend a pre-consultation meeting with the Township, Region of Niagara and NPCA for formal pre-consultation comments.

The subject property has been designated as being within the Northwest Quadrant Secondary Plan approved in June of 2017 without appeal. This plan outlines the land use designations, densities, road patterns, pedestrian trails and linkages, water, wastewater and stormwater servicing, community design, phasing and implementation.

The following is a summary of initial planning comments based on the materials submitted:

**Transportation**

*Street Pattern*

The proposed street pattern does not fully match with the approved street pattern on the secondary plan. The connections of Street G and F to South Grimsby Road 5, in alignment with Westlea Drive and Northridge Drive have been provided as well as Street H and Street G to South Grimsby Road 6. Street F does not extend the full length as shown on the secondary plan. Although there is flexibility in the Secondary Plan, Adequate planning and engineering justification will be required before the submitted version will be supported by staff.

*Trail Network*

It appears that a portion of land along the rail line has been created as a block to accommodate a trail. A connection to this trail midway through the development should be provided to allow for pedestrian access. Noise attenuation berms and fences, if transferred to the Township, will need to be designed with slopes that allow for regular maintenance and a noise wall with an extended lifespan, or alternatively an acceptable design that can be naturalized.

### *Access*

Currently there appears to be only one access that the applicant is able to construct due to the property parcel fabric and ownership. A secondary access to a public street should be provided. Roads that will temporarily dead end will require temporary cul-de-sacs until the remainder of the streets are constructed.

A temporary access using the Greek Community Hall's driveway was requested, however, the Region is not supportive of the request, and are advising that any road access (permanent or temporary) should be directly aligned with Streamside Drive.

### *South Grimsby Road 5*

South Grimsby Road 5 will need to be upgraded to a full urban standard. The developer would only be responsible for one side of the right of way.

### **Land Use Designations**

The proposed plan appears to show 54 single detached dwelling lots (with a possible 16 more on the Greek Community Hall lands) and 147 condominium townhouses. The total number of units proposed are 201 units with an approximate density of 19.4 units per hectare. At 2.8 people per unit, this maintains the minimum density requirement of 50 people per hectare in the Township Official Plan. The proposed plan also appears to have an area for parkland and for a SWM pond. Single Detached, Semi Detached and Townhouses are all permitted within the low density and medium density designations. Generally this proposed development has a good mix of single detached dwellings and townhouses, however, the applicant should consider the addition of low rise apartment buildings to provide more affordable housing and a slight increase in density towards 60 people and jobs would be preferred.

### *Low Density Residential*

The proposed low density residential zone is not exactly lined up with the low density residential designation as shown on the secondary plan. The low density zone is slightly smaller than that shown on the secondary plan. The low density zone appears to be all single detached dwellings which is acceptable to planning staff. The low density area should be enlarged to match closer to the secondary plan. Low density zones should not exceed a maximum density of 30 units per hectare.

The maximum building height of the low density designation is 2.5 storeys. All development which is within the low density designation must not exceed 2.5 storeys without an amendment to the Secondary Plan.

### *Medium Density Residential*

The proposed medium density zone covers more area than what is shown in the secondary plan. Generally, the layout of the units within the medium density residential areas are acceptable, however more information and detail is needed with regards to these blocks (i.e. parking requirements, driveway widths, unit types, elevations, etc.) The medium density zone should not exceed 45 units per hectare and shall not exceed four storeys.

### *Stormwater Management*

The Stormwater management pond is located in approximately in the same location as shown on the secondary plan. It abuts Regional Road 20 and Street H. The Regions Urban Design Guidelines recommend that stormwater facilities be designed to avoid fencing requirements and be accessible for the public open spaces while maintaining safety. The Secondary Plan also recommends that stormwater management facilities be integrated within the park to create a more usable public open space. These items should be integrated in the stormwater management facilities design as to improve its functionality and aesthetics from Regional Road 20.

### *Parks and Trail Linkages*

The proposed plan appears to show a trail corridor along the rail line however it is not labelled and the trail is not shown. A trail connection should also be included midway through the development, from Street G to the rail trail corridor.

The park is in the appropriate area and appears to be the appropriate size and complies with the intent of the secondary plan.

### **Urban Design**

An urban design brief will be required as part of a draft plan of subdivision application, and a full urban design report will be required as a condition prior to full subdivision approval.

### **CP Rail Separation**

Please refer to the CP Railway Principle Main Line Requirements attached to this memo for full details regarding development adjacent to a principle main line. All dwellings (not including garages) must be a minimum of 30 metres from the rail right of way.

### **Zoning Bylaw Amendment**

The property is currently zoned Development 'D'. The lands will have to be rezoned to both a low density, a medium density residential zone and an open space zoning. The rezoning should reflect the designations as laid out in the Secondary Plan (Low density residential designation should receive a low density zone appropriate for the development, and medium density designation a medium density zoning).

The development should comply with the appropriate zone designation without the need for site specific exceptions.

### **Other Studies**

Further studies will be required with submission of a draft Plan of Subdivision application. These will be determined at a pre-consultation meeting but would likely include:

- Planning Justification Report
- Conceptual Site Plan
- Land Use/Market Needs Report
- Detailed Stormwater Management Study
- Archaeological Assessment

- Environmental Impact Study
- Transportation Impact Study
- Air Quality/Noise Study
- Vibration Study
- Detailed Servicing Design Studies
- Detailed Road Design Studies
- Grading Plans
- Urban Design Reports
- Landscape Plans
- Tree Preservation Plans
- Geotechnical Report
- Functional Servicing Report
- Record of Site Condition
- Environmental Site Assessment
- Hydrogeological Study

## **Conclusion**

This development exceeds the overall density required for greenfield development within the greenfield area (50 people per hectare) at approximately 54 people per hectare (2.8 people per unit). However, there are some variations in the proposed plan which do not reflect what is in the Secondary Plan. These differences include:

1. An altered street pattern which impacts the low density designation (Street F)
2. Adjustments to the low and medium density designation (5.2 & 5.3)
3. A trail connection midway through the development from Street G.
4. Road access to South Grimsby Road 5 and South Grimsby Road 6.

Additional items of consideration include integrating SWM features into a public park, potential setback deficiencies, water supply/pressure for fire flows and sanitary sewer capacity.

A formal pre-consultation meeting is required to fully determine the appropriate studies and reports required as part of a complete application.

These are preliminary comments prepared by Township staff with limited information on the development and are therefore subject to change once more information is provided through a complete application. If you have any questions please contact either Brian Treble (905-957-5138) or myself (905-957-5133).

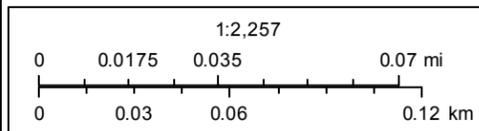
Gerrit Boerema,  
Planner II

Brian Treble,  
Director of Planning and Building





- Corporate Watershed Divide NPCA
- NPCA APPROXIMATE REGULATION LANDS
- Lyons Creek East Administrative Controls Area
- Ontario Road Network**
  - Major Highways
  - Arterial Roads
  - Local Roads
  - Railway Network
  - Assessment Parcels - Cityview
  - 2K Hydrography
  - Natural Areas (Large Scale)
  - Red: Band\_1
  - Green: Band\_2
  - Blue: Band\_3



4/3/2019, 10:26:06 AM

This map is for illustrative purposes only. Information contained hereon is not intended to constitute advice, is not suitable for professional review or a site survey, and is subject to change without notice. The NPCA takes no responsibility for, nor guarantees, the accuracy of the information contained on this map. Any interpretations or conclusions drawn from this map are the sole responsibility of the user. THIS IS NOT A PLAN OF SURVEY.

Author:  
Notes

# PART 6. RESIDENTIAL ZONES

## 6.1 APPLICABLE ZONES

The permitted *uses* and regulations of Part 6 apply to land within the following *zones*:

<b>Zone</b>	<b>Symbol</b>
Rural Residential	RuR
Residential Low Density	– Type 1A R1A
	– Type 1B R1B
	– Type 1C R1C
	– Type 1D R1D
	– Type 2 R2
	– Type 3 R3
Residential Medium Density	– Type 1 RM1
	– Type 2 RM2
	– Type 3 RM3
Residential High Density	RH

These *zones* apply to land identified with the corresponding *zone* symbol as shown in Schedule “A”.

## 6.2 PERMITTED USES

In the *zones* identified in Section 6.1, no *person* shall *use* or permit the *use* of any *lot* or *erect, alter* or *use* any *building* or *structure* for any purpose except in accordance with the permitted *uses* in Table 13.

**Table 13: Permitted Uses in Residential Zones**

Uses	Zones where Permitted											
<b>Principal Uses</b>												
<i>Apartment dwelling</i>											RM3	RH
<i>Duplex dwelling</i>										RM2		
<i>Fourplex dwelling</i>										RM2	RM3	
<i>Retirement home</i>												RH
<i>Semi-detached dwelling</i>						R2	R3	RM1	RM2	RM3		
<i>Single detached dwelling</i>	RuR	R1A	R1B	R1C	R1D	R2	R3					
<i>Stacked townhouse dwelling</i>											RM3	
<i>Street townhouse dwelling</i>									RM1	RM2	RM3	
<i>Townhouse dwelling</i>										RM2	RM3	
<i>Triplex dwelling</i>										RM2	RM3	
<b>Accessory Uses <sup>(1)</sup></b>												
<i>Accessory buildings or structures and accessory uses (see s. 3.1)</i>	RuR <sup>(1)</sup>	R1A <sup>(1)</sup>	R1B <sup>(1)</sup>	R1C <sup>(1)</sup>	R1D <sup>(1)</sup>	R2 <sup>(1)</sup>	R3 <sup>(1)</sup>	RM1 <sup>(1)</sup>	RM2 <sup>(1)</sup>	RM3 <sup>(1)</sup>	RH <sup>(1)</sup>	
<i>Accessory dwelling unit (see s. 3.2)</i>	RuR <sup>(1)</sup>	R1A <sup>(1)</sup>	R1B <sup>(1)</sup>	R1C <sup>(1)</sup>	R1D <sup>(1)</sup>	R2 <sup>(1)</sup>	R3 <sup>(1)</sup>					
<i>Bed and breakfast establishment (see s. 3.4)</i>	RuR <sup>(1)</sup>	R1A <sup>(1)</sup>	R1B <sup>(1)</sup>	R1C <sup>(1)</sup>	R1D <sup>(1)</sup>							

Uses	Zones where Permitted										
<i>Boarding or rooming house</i>	RuR <sup>(1)</sup>	R1A <sup>(1)</sup>	R1B <sup>(1)</sup>	R1C <sup>(1)</sup>	R1D <sup>(1)</sup>						RM3 <sup>(1)</sup>
<i>Garden suite</i> (see s. 3.2)	RuR <sup>(1)</sup>	R1A <sup>(1)</sup>	R1B <sup>(1)</sup>	R1C <sup>(1)</sup>	R1D <sup>(1)</sup>						
<i>Group home</i> (see s. 3.6)	RuR <sup>(1)</sup>	R1A <sup>(1)</sup>	R1B <sup>(1)</sup>	R1C <sup>(1)</sup>	R1D <sup>(1)</sup>	R2 <sup>(1)</sup>	R3 <sup>(1)</sup>				
<i>Home occupation</i> (see s. 3.7)	RuR <sup>(1)</sup>	R1A <sup>(1)</sup>	R1B <sup>(1)</sup>	R1C <sup>(1)</sup>	R1D <sup>(1)</sup>	R2 <sup>(1)</sup>	R3 <sup>(1)</sup>				
<i>Renewable energy system</i> (see s. 3.15)	RuR <sup>(1)</sup>	R1A <sup>(1)</sup>	R1B <sup>(1)</sup>	R1C <sup>(1)</sup>	R1D <sup>(1)</sup>	R2 <sup>(1)</sup>	R3 <sup>(1)</sup>	RM1 <sup>(1)</sup>	RM2 <sup>(1)</sup>	RM3 <sup>(1)</sup>	RH <sup>(1)</sup>

<sup>(1)</sup> Denotes *uses* that are only permitted accessory to or in conjunction with a permitted *principal use*.

### 6.3 REGULATIONS

In the *zones* identified in Section 6.1, no *person* shall *use* or permit the *use* of any *lot* or *erect, alter* or *use* any *building* or *structure* for any purpose except in accordance with the regulations in Tables 14 and 15.

**Table 14: Regulations for Permitted Uses in Low Density and Rural Residential Zones**

Regulation			Zone Requirements							
			RuR	R1A	R1B	R1C	R1D	R2	R3	
Minimum lot area	Municipal services	<i>Single detached dwelling</i>	0.4 ha	800m <sup>2</sup>	550m <sup>2</sup>	475m <sup>2</sup>	400m <sup>2</sup>	360m <sup>2</sup>	300m <sup>2</sup>	
		<i>Semi-detached dwelling</i> <sup>(1)</sup>	-					720m <sup>2</sup>	450m <sup>2</sup>	
	Private services		0.4 ha	-						
Minimum lot frontage	Municipal services	<i>Corner lot</i>	<i>Single detached dwelling</i>	45m	21m	18m	15m	15m	12.5m	
			<i>Semi-detached dwelling</i> <sup>(1)</sup>	-					24m	18m
		<i>Other lot</i>	<i>Single detached dwelling</i>	45m	21m	18m	15m	13m	12m	10m
			<i>Semi-detached dwelling</i> <sup>(1)</sup>	-					24m	18m
	Private services		45m	-						
Minimum front yard	<i>Dwelling</i>		15m	7.5m	6m	4.5m				
	<i>Private garage</i>					6m				
Minimum exterior side yard			9m	6m	5m	4.5m		3m		
Minimum interior side yard	<i>Dwelling</i>		3m		1.5m	1.2m		1.2m <sup>(2)</sup>		
	<i>Private garage</i> (attached to <i>dwelling</i> )							0.6m <sup>(3)</sup>		
Minimum rear yard			15m	10m		7.5m				
Maximum lot coverage	Municipal services		20%	35%	40%	45%				
	Private services			20%	-					
Maximum height			10m							
Minimum landscaped open space			35%	30%	25%					

- <sup>(1)</sup> Where each *dwelling unit* of a *semi-detached dwelling* is located on a separate *lot*, the minimum *lot area* and minimum *lot frontage* for each *dwelling unit* shall be half the requirement identified above for the applicable *zone*.
- <sup>(2)</sup> Where each *dwelling unit* of a *semi-detached dwelling* is located on a separate *lot*, no *interior side yard* shall be required along the common *lot line* of the attached wall joining the two *dwelling units*.
- <sup>(3)</sup> This provision shall only apply where an attached *private garage* is located on a *lot* where the nearest *interior side yard* on the adjoining *lot* is required to be 1.2 metres or greater in width. If the nearest *interior side yard* on the adjoining *lot* is permitted to be less 1.2 metres, the minimum *interior side yard* for the *private garage* shall be 1.2 metres.

**Table 15: Regulations for Permitted Uses in Medium and High Density Residential Zones**

Regulation		Zone Requirements			
		RM1	RM2	RM3	RH
Minimum lot area (per dwelling unit)	Apartment dwelling	-		160m <sup>2</sup>	120m <sup>2</sup>
	Duplex dwelling	-	250m <sup>2</sup>	-	
	Fourplex dwelling	-		180m <sup>2</sup>	-
	Retirement home	-			120m <sup>2</sup>
	Semi-detached dwelling <sup>(1)</sup>	270m <sup>2</sup>	200 m <sup>2</sup>		-
	Stacked townhouse dwelling	-	-	160m <sup>2</sup>	-
	Street townhouse dwelling	225m <sup>2</sup>	180m <sup>2</sup>		-
	Townhouse dwelling	-	200m <sup>2</sup>	180m <sup>2</sup>	-
	Triplex dwelling	-	250m <sup>2</sup>	200m <sup>2</sup>	-
Minimum lot frontage <sup>(2)</sup>	Apartment dwelling	-		30m	
	Duplex dwelling	-	20m	-	
	Fourplex dwelling	-	30m		-
	Retirement home	-			30m
	Semi-detached dwelling <sup>(1)</sup>	9m/unit	8m/unit		-
	Stacked townhouse dwelling	-	-	30m	-
	Street townhouse dwelling	7.5m/unit	6m/unit		-
	Townhouse dwelling	-	30m		-
	Triplex dwelling	-	20m	18m	-
Minimum front yard	Dwelling	4.5m			7.5m
	Private garage	6m			
Minimum exterior side yard		3m			Greater of 50% of building height or 3m
Minimum interior side yard	Adjoining a lot in a low density residential zone	3m <sup>(3)</sup>			
	Adjoining a lot in any other zone	1.2m <sup>(3)</sup>			3m
Minimum rear yard <sup>(4)</sup>	Adjoining a lot in a low density residential zone	7.5m			
	Adjoining a lot in any other zone	6m			
Maximum lot coverage		45%	50%		
Minimum separation distance between dwellings on the same lot	Between exterior side walls	-	3m		
	Between exterior front or rear walls		12m		
	Between exterior front or rear walls and side walls		7.5m		
Maximum height		10m	12m		15m
Minimum landscaped open space		25%			
Minimum amenity area <sup>(5)</sup>	Dwelling with 3 or 4 dwelling units on one lot	-	20m <sup>2</sup> per dwelling unit		
	Dwelling with 5 to 8 dwelling units on one lot		40m <sup>2</sup> plus 10m <sup>2</sup> per dwelling unit		
	Dwelling with 9 or more dwelling units on one lot		-	80m <sup>2</sup> plus 5.5m <sup>2</sup> per dwelling unit	

- (1) Where *semi-detached dwellings* are located in the RM2 or RM3 zone, the *dwelling units* shall be located on lands within a Registered Plan of Condominium or shall be tied to a common elements condominium *private street*.
- (2) Where multiple attached *dwellings* are located on the same *lot* in the RM2 or RM3 Zone, including more than one type of attached *dwelling*, the minimum *lot frontage* requirement of the RM2 or RM3 zone, as applicable, shall be 30 metres in the case of a *lot* that contains one or more *fourplex* and/or *townhouse dwelling* and/or *stacked townhouse dwelling*, and 20 metres in all other cases, and shall apply to the entire *lot*. For *semi-detached dwellings* where each unit is located on a separate *lot*, and for *street townhouse dwellings*, each *lot* shall meet the prescribed minimum *lot frontage*.
- (3) Where each *dwelling unit* of a *semi-detached dwelling* is located on a separate *lot*, and for *street townhouse dwellings*, no *interior side yard* shall be required along the common *lot line* of the attached wall joining two *dwelling units*.
- (4) No rear yard is required for a *back-to-back townhouse dwelling*.
- (5) No common outdoor *amenity area* provided at grade shall have an area less than 40m<sup>2</sup>.

# Appendix F – Draft Zoning By-law Amendment

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THE CORPORATION OF THE TOWNSHIP OF WEST LINCOLN

**BY-LAW NO. 2020-XXXX**

**A BY-LAW TO AMEND ZONING BY-LAW NO. 2017-70,  
AS AMENDED, OF THE TOWNSHIP OF WEST LINCOLN**

**WHEREAS THE TOWNSHIP OF WEST LINCOLN COUNCIL IS EMPOWERED TO ENACT THIS BY-LAW BY VIRTUE OF THE PROVISIONS OF SECTION 34 OF THE PLANNING ACT R.S.O., 1990;**

1. **THAT** Schedule 'A' Map S1 to Zoning By-law 2017-70, as amended, is hereby amended by changing the zoning on Part of Lot 9 and Plan M98 in the Township of West Lincoln, Regional Municipality of Niagara, shown as the subject lands on Schedule A, attached hereto and forming part of this By-law.
2. **THAT** Schedule 'A' Map S1 to Zoning By-law 17-70, as amended, is hereby amended by changing the zoning on part of the subject lands shown on Zoning Schedule A, attached hereto and forming part of this By-law from a "Development" (D) Zone to "Residential Low Density Type 3" (R3-X) Modified Zone.
3. **THAT** Schedule 'A' Map S1 to Zoning By-law 17-70, as amended, is hereby amended by changing the zoning on part of the subject lands shown on Zoning Schedule A, attached hereto and forming part of this By-law from a "Development" (D) Zone to a "Residential Medium Density Type 3" (RM3-X) Modified Zone.
4. **THAT** Schedule 'A' Map S1 to Zoning By-law 17-70, as amended, is hereby amended by changing the zoning on part of the subject lands shown on Zoning Schedule A, attached hereto and forming part of this By-law from a "Development" (D) Zone to a "Residential Medium Density Type 3" (RM3-XX) Modified Zone.
5. **THAT** Schedule 'A' Map S1 to Zoning By-law 17-70, as amended, is hereby amended by changing the zoning on part of the subject lands shown on Zoning Schedule A, attached hereto and forming part of this By-law from a "Development" (D) Zone to a "Residential Medium Density Type 3" (RM3-XXX) Modified Zone.
6. **THAT** Schedule 'A' Map S1 to Zoning By-law 17-70, as amended, is hereby amended by changing the zoning on part of the subject lands shown on Zoning Schedule A, attached hereto and forming part of this By-law from a "Development" (D) Zone to a "Service Commercial" (C3-X) Modified Zone.
7. **THAT** schedule "A" Map S1 to Zoning By-law 17-70, as amended, is hereby amended by change the zoning on part of the subject lands shown on Zoning Schedule A, attached hereto and forming part of this By-law from a "Development" (D) Zone to "Open Space" (OS) Zone.
8. **THAT** Part 6 of Zoning By-law 17-70, as amended, is hereby amended by adding the following to part 13.2 Site-Specific Provisions:

R3-X            Notwithstanding the Minimum Lot Area for a Single Detached dwelling with Municipal Services and Minimum Lot Frontage for a Single Detached dwelling on a corner lot with Municipal Services, provisions of the "Low Density and Rural Residential Zones" in Table 14 of Section 6.3, the part of the subject lands shown as R3-X on Schedule 'A' of Zoning By-law 2017-70 shall be subject to the following amended provisions:

- (a) Minimum Lot Area of a Single detached dwelling on Municipal Services shall be 290m<sup>2</sup> instead of the required 300m<sup>2</sup>.

(b) Minimum Lot Frontage of a Corner Lot on Municipal Services for a Single Detached dwelling shall be 12.0m instead of 12.5m.

9. **THAT** Part 6 of Zoning By-law 17-70, as amended, is hereby amended by adding the following to part 13.2 Site-Specific Provisions:

RM3-X Notwithstanding the Minimum Lot Area, Minimum Lot Frontage, Minimum Interior Side Yard that adjoins a lot in a low density residential zone, Minimum Rear Yard that adjoins a lot in a low density residential zone and Minimum Amenity Area for a dwelling with 5 to 8 dwelling units on one lot provisions of the "Medium and High Density Residential Zones" in Table 15 of Section 6.3, the part of the subject lands shown as RM3-X on Schedule 'A' of Zoning By-law 2017-70 shall be subject to the following amended provisions:

- (a) Minimum Lot Area (per dwelling unit) for a Street Townhouse shall be 125m<sup>2</sup>, instead of the required 180m<sup>2</sup>,
- (b) Minimum Lot Frontage for a Street Townhouse Dwelling shall be 5.1m/unit instead of the required 6.0m/unit,
- (c) Minimum Interior Side Yard for a Street Townhouse that is adjoining lot in a low density residential zone shall be 1.0m instead of the required 3.0m,
- (d) Minimum Rear Yard for a Street Townhouse that is adjoining lot in a low density residential zone shall be 7.0 m, instead of the required 7.5 m
- (e) Minimum Amenity Area shall not apply for Street Townhouses

10. **THAT** Part 6 of Zoning By-law 17-70, as amended, is hereby amended by adding the following to part 13.2 Site-Specific Provisions:

RM3-XX Notwithstanding the Minimum Lot Area per Dwelling Unit, and Minimum Rear Yard that adjoins a lot in a low density residential zone and Minimum Amenity Area for a dwelling with 5 to 8 dwelling units provisions of the "Medium and High Density Residential Zones" in Table 15 of Section 6.3, the part of the subject lands shown as RM3-XX on Schedule 'A' of Zoning By-law 2017-70 shall be subject to the following amended provisions:

- (a) Minimum Lot Area (per dwelling unit) for a Townhouse shall be 155m<sup>2</sup>, instead of the required 180m<sup>2</sup>,
- (b) Minimum Rear Yard for a Street Townhouse that is adjoining lot in a low density residential zone shall be 6.9m, instead of the required 7.5m

11. **THAT** Part 6 of Zoning By-law 17-70, as amended, is hereby amended by adding the following to part 13.2 Site-Specific Provisions:

RM3-XXX Notwithstanding the Minimum Lot Area per Dwelling Unit, Minimum Rear Yard that adjoins a lot in another zone, Maximum Lot Coverage, Minimum Landscape Open Space and Minimum Amenity Area for a dwelling with 9 or more dwelling units on one lot provisions of the "Medium and High Density Residential Zones" in Table 15 of Section 6.3, the part of the subject lands shown as RM3-XXX on Schedule 'A' of Zoning By-law 2017-70 shall be subject to the following amended provisions:

- (a) Minimum Lot Area (per dwelling unit) for a Back-to-Back Townhouse shall be 95m<sup>2</sup>, instead of the required 180m<sup>2</sup>,

- (b) Minimum Rear Yard for a Back-to-Back Townhouse that is adjoining lot in another zone shall be 0.0m, instead of the required 6.0m,
- (c) Maximum Lot Coverage for a Back-to-Back townhouse shall be 58% instead of the required 50%;
- (d) Minimum Landscape Open Space for a Back-to-Back Townhouse shall be 15% instead of the required 25%
- (e) Minimum Amenity Area for a Back-to-Back dwelling with 9 or more dwelling units on one lot shall be 35m<sup>2</sup> instead of the required 85.5m<sup>2</sup>.

12. **THAT** Part 7 of Zoning By-law 17-70, as amended, is hereby amended by adding the following to part 13.2 Site Specific Provisions:

C3-X In addition to the Permitted Uses listed in Table 16 of Section 7.2 Permitted Uses, the part of the subject lands shown as C3-X on Schedule 'A' of Zoning By-law 2017-70, the following shall also be permitted:

- (a) Apartment Dwelling
- (b) Temporary Sales Trailer

Notwithstanding the Minimum front yard provision that adjoins Regional Road 14 or 20 of the "Regulations for Permitted Uses in Commercial Zones" in Table 17 of Section 7.3 Regulations, the part of the subject lands shown as C3-X on Schedule 'A' of Zoning By-law 2017-70 shall be subject to the following amended provisions:

- (a) Minimum front yard adjoining Regional Road 14 or 20 shall be 3.0m instead of the required 9.0m.

Notwithstanding Table 6 "Required Parking Facilities" of Section 3.12.6, the part of the subject lands shown as C3-X on Schedule 'A' of Zoning By-law 2017-70 shall be subject to the following amended provision:

- (a) 42 parking space shall be require for the mixed use building consisting of 32 dwelling units and approximately 1300m<sup>2</sup> of commercial space.

13. **THAT** Part 3 of Zoning By-law 17-70, as amended, is hereby amended by adding the following to part 13.2 Site-Specific Provisions.

Notwithstanding provisions (b) Table 3 regarding Double traffic lane for travel in one or two directions for Driveways for Residential uses with five (5) or more dwelling units and (e) (ii) of Section 3.12.2, the subject lands shown on Schedule 'A' of Zoning By-law 2017-70 shall be subject to the following amended provisions:

- (b) Minimum width for a double traffic lane for travel in one or two directions for driveways for residential use with five (5) or more dwelling units shall be 6m instead of the required 7.5m.
- (e) li. A driveway for a residential use with five (5) or more dwelling units or for a non-residential use shall have a minimum setback of 0.0m instead of the required 3.0m to a lot line that adjoins a lot within a Residential zone.

14. **AND THAT** this By-law shall become effective from and after the date of the passing thereof.

**READ A FIRST, SECOND AND THIRD  
TIME AND FINALLY PASSED THIS  
\_\_\_ DAY OF \_\_\_\_\_, 2020.**

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MAYOR

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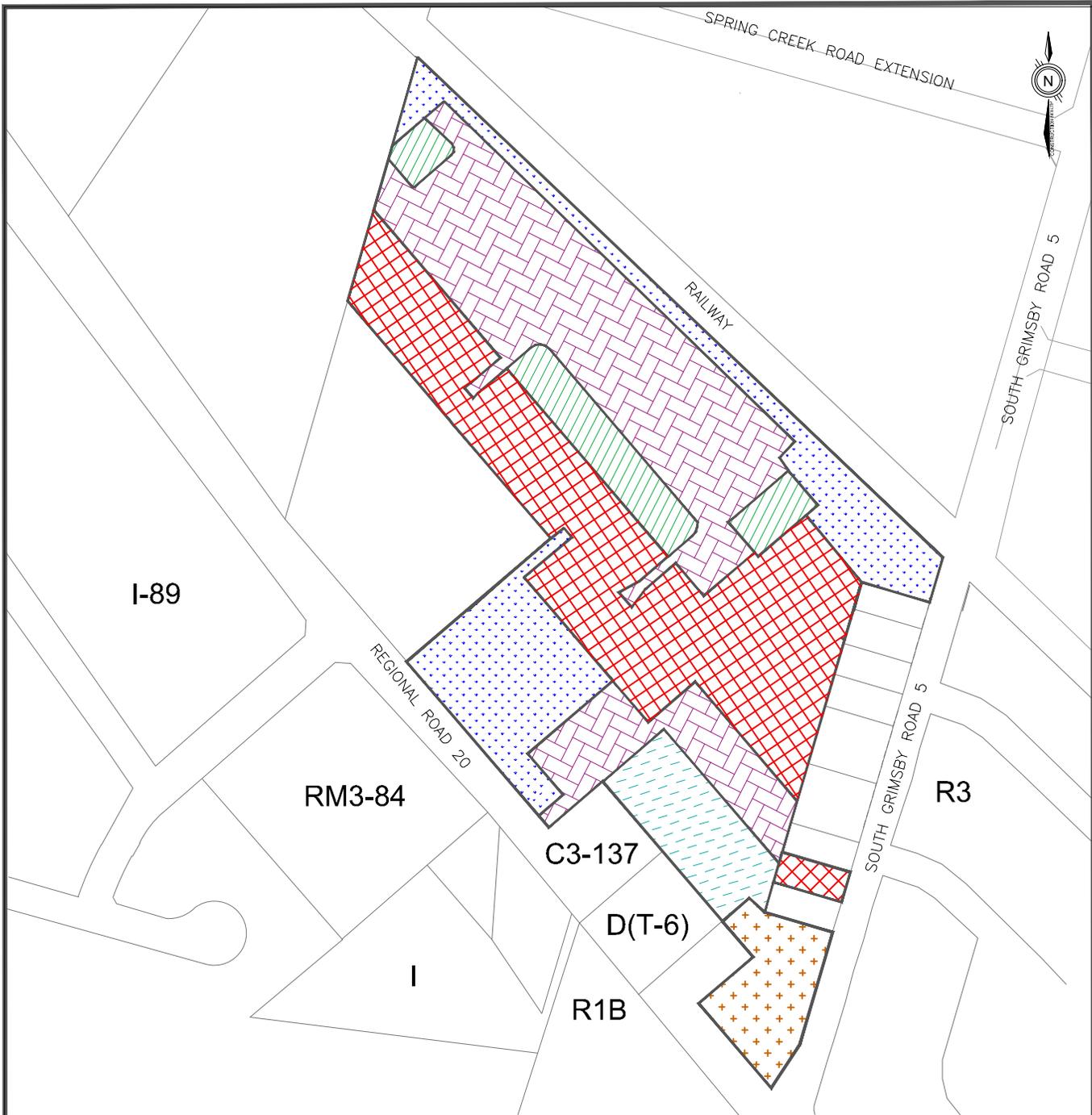
CLERK

## **EXPLANATION OF THE PURPOSE AND EFFECT OF BY-LAW NO. 2020-XXXX**

The subject lands are located west of South Grimsby Road 5, south of the Canadian Pacific Rail Line and north of Regional Road 20. The lands are legally described as Part of Lot 9 in Plan M98 as confirmed by Plan 30BA1695 in the Geographic Township of West Lincoln, Regional Municipality of Niagara.

The subject lands are currently zoned 'Development' (D). The purpose of the zoning by-law is to rezone the subject lands as per the proposed Draft Plan of Subdivision, and more particularly to Residential Low Density Type 3 (R3-X) Modified, Residential Medium Density Type 3 (RM3-X) Modified, Residential Medium Density Type 3 (RM3-XX) Modified, Residential Medium Density Type 3 (RM3-XXX) Modified, Service Commercial (C3-X) Modified and Open Space (OS) to intensify the use of the lands in accordance with the Growth Management Strategy and Northwest Quadrant Secondary Plan.

File: \_\_\_\_\_  
Applicants: IBI Group



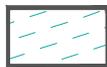
To rezone from Development "D" Zone to Residential Low Density "R3-X" Modified Zone.



To rezone from Development "D" Zone to Residential Medium Density Type 3 "RM3-X" Modified Zone.



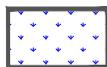
To rezone from Development "D" Zone to Residential Medium Density Type 3 "RM3-XX" Modified Zone.



To rezone from Development "D" Zone to Residential Medium Density Type 3 "RM3-XXX" Modified Zone.



To rezone from Development "D" Zone to Service Commercial "C3-X" Zone.



To rezone from Development "D" Zone to Open Space "OS" Zone.

# Township of West Lincoln

## Schedule A

### Zoning By-law No. 2017-70

# S1

This is Schedule 'A' to By-law No. 2020-\_\_ passed  
 \_\_ day of \_\_\_\_\_ 2020.

# Appendix G – Planning Committee Meeting Minutes

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# MEMO

KITCHENER  
WOODBRIDGE  
LONDON  
KINGSTON  
BARRIE  
BURLINGTON

<b>To:</b>	<b>Brian Treble</b>
<b>From:</b>	<b>Dan Currie</b>
<b>Date:</b>	<b>June 9, 2017</b>
<b>File:</b>	<b>08234Q</b>
<b>Subject:</b>	<b>Secondary Plan for the Northwest Quadrant Response to IBI Comments and Inquires</b>

We have reviewed the comments provided by John Ariens' of IBI group, on behalf of his clients Marz Homes and Dunloe Developments, with respect to the recommended final land use schedule and implementing secondary plan policies for the Northwest Quadrant and offer the following responses:

- The revised Secondary Plan provides allows for development of medium density residential and low density residential uses within each designation (i.e. low density residential uses are permitted the medium density residential designation, subject to the applicant demonstrating that the overall density target can be achieved. Similarly, medium density residential uses are permitted in the low density residential designation). These changes have been made to allow for flexible development and address comments and concerns raised by multiple landowners and their representatives
- The location of the SWM pond shown on the land use schedule is based on work completed by MTE and represents the most cost-effective and appropriate location, given topography of the lands and the required connections to outlets along the CP Rail line. The land use schedule also identifies linkages as part of the linear park which would serve as connections from the outlet to the SWM pond
- The road pattern shown on the land use schedule provides connections to the existing roads in adjacent subdivisions. The location of the roads has been identified as such to ensure that there are not off-setting streets connecting existing and future development.
- The secondary plan considers that an extension of Spring Creek Road may be built in stages. Portions of the road must be built to allow frontage to develop; however, the Plan does not require that the entirety of the road be built prior to development
- Policies have been added to the Secondary Plan to allow for the Township to consider alternate design of the Future Development designation. This change has been made to allow for flexible

development and high quality urban design and provides consideration for potential future urban expansion of the lands to the north.

Please do not hesitate to contact me if you have any questions related to the responses contained herein.